

A66 Northern Trans-Pennine Project TR010062

Application for a Non-Material Change to The A66 Northern Trans-Pennine Development Consent Order 2024

Application Statement

Planning Act 2008

**Infrastructure Planning (Changes to, and
Revocation of, Development Consent Orders) Regulations 2011**

10 December 2025

Infrastructure Planning

Planning Act 2008

The Infrastructure Planning (Changes to, and Revocation of, Development Consent Orders) Regulations 2011
(as amended)

Non-Material Change Application to
The A66 Northern Trans-Pennine
Development Consent Order 2024

APPLICATION STATEMENT

Regulation	4
Planning Inspectorate Scheme Reference	TR010062
Author:	A66 Northern Trans-Pennine Project, Project Team, National Highways

Version	Date	Status of Version
Rev 1	10 Dec 2025	Non-Material Change Application

CONTENTS

1.	Introduction	4
1.1.	Overview	4
1.2.	The context for the Proposed Changes	7
2.	The Proposed Non-Material Changes	8
2.1.	General	8
2.2.	Proposed Change 1 - Cross Street	8
2.3.	Proposed Change 2 - Green Lane	14
2.4.	Proposed Change 3 - Long Marton	18
2.5.	Proposed Change 4 - Access track at Kirkby Thore	23
2.6.	Proposed Change 5 – B6542 and Spitals Farm Access	27
2.7.	Proposed Change 6 - Kemplay Bank	30
2.8.	Proposed Change 7 – B6262 Combined Public Rights of Way and Private Means of Access	32
2.9.	Proposed Change 8 – Powis House Private Means of Access	36
3.	Assessment of the Proposed Non-Material Changes	41
4.	Pre-application Engagement	47
5.	Conclusion	50

Annex

Annex A. Parties to be consulted on the application
Annex B. Compiled list of Proposed Amended Drawings
Annex C. Consultation Evidence

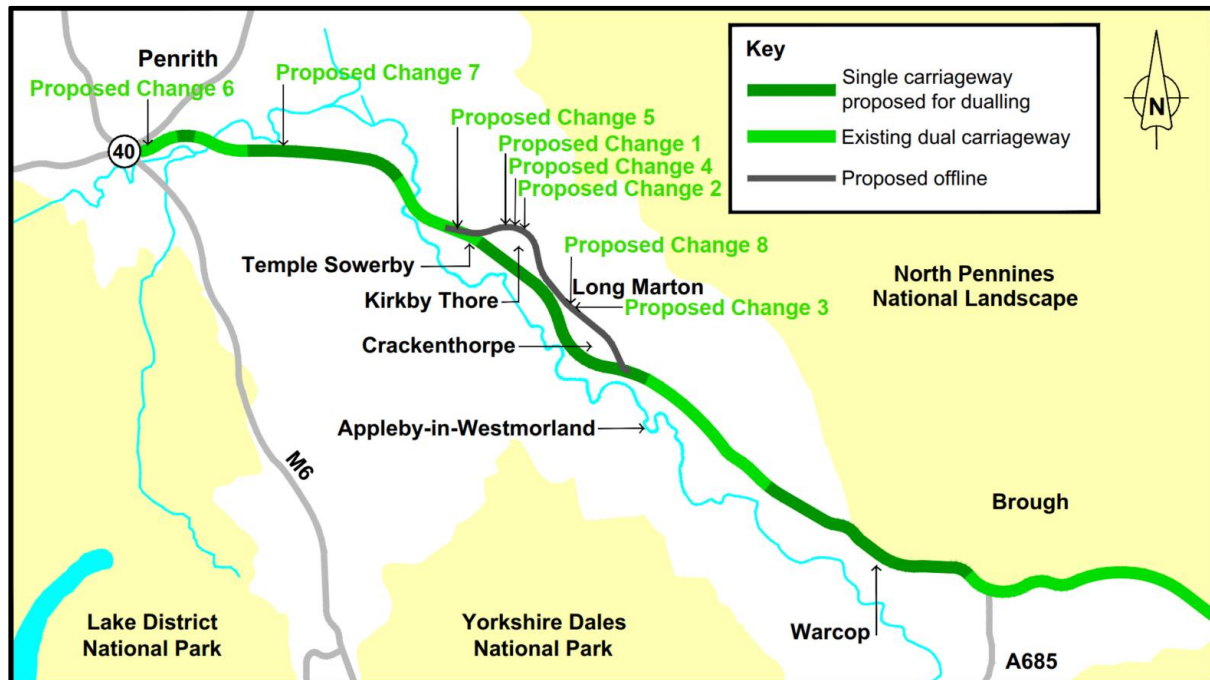
1. Introduction

1.1. Overview

- 1.1.1. National Highways ('the Applicant') has the benefit of the A66 Northern Trans-Pennine Development Consent Order (DCO) 2024 (SI.2024 No.360), hereafter referred to as 'the Order' which was made by the Secretary of State for Transport on 7th March 2024, and came into force on 28th March 2024.
- 1.1.2. The Order authorises the construction, operation and maintenance of the A66 Northern Trans-Pennine Project ('the Project'), which comprises the dualling of all remaining single lengths of carriageway of the A66 to create a continuous 70mph dual carriageway (with the exception of a short length of 50mph dualling between M6 Junction 40 and east of Kemplay Bank) across the North Pennines between M6 junction 40 at Penrith and A1(M) at Scotch Corner, including improvement of these two junctions ('the Authorised Development').
- 1.1.3. The existing route is 80km in length and intermittently dualled with approximately 30km of single carriageway, within six separate sections. The Authorised Development comprises eight schemes (known as Scheme 0102, Scheme 03, Scheme 0405, Scheme 06, Scheme 07, Scheme 08, Scheme 09 and Scheme 11 respectively) with the following main features:
- upgrades to the existing single carriageway sections of the road to dual carriageway;
 - junction improvements; and
 - minor improvements to the existing dual carriageway sections.
- 1.1.4. This Application Statement has been prepared to accompany an application for a non-material change to the Order ('the Change Application'). The application is made pursuant to section 153 and paragraph 2 of Schedule 6 to the Planning Act 2008, and in accordance with the Infrastructure Planning (Changes to, and Revocation of, Development Consent Orders) Regulations 2011, as amended ('the Regulations').
- 1.1.5. Consent is sought for the following eight 'Proposed Changes' to the Order ('the Proposed Changes'):
- **Proposed Change 1 - Cross Street:** amendments to the horizontal alignment of Cross Street
 - **Proposed Change 2 - Green Lane:** amendment to the horizontal alignment of Green Lane
 - **Proposed Change 3 - Long Marton:** amendment to the alignment of Morland Road to reduce bridge span and amendment to the alignment and cross section of Long Marton bridge

- **Proposed Change 4 - Access track at Kirkby Thore:** removal of a Private Means of Access (PMA) track through farmland
- **Proposed Change 5 - B6542 and Spitals Farm Access:** amendment to the alignment of the B6542 and reduction in length of the Private Means of Access (PMA) underpass
- **Proposed Change 6 - Kemplay Bank:** amendment to DCO centreline of part of Work Number 0102-1E
- **Proposed Change 7 - B6262 Combined Public Rights of Way and Private Means of Access:** realignment of various lengths of combined Public Rights of Way (PRoW) and Private Means of Access (PMA):
 - on the south side of the A66, running between the B6262 and the site of the former Llama Karma Kafe, and providing access to the Countess Pillar which is located between them (comprising part of Work No. 3-7A, and both Work Nos. 03-9A and 03-9B); and
 - crossing over the A66, then running along the north side of the A66 in an easterly direction (comprising part of Work No. 03-7A and part of Work No. 03-7B)
- **Proposed Change 8 - Powis House Private Means of Access:** realignment of footpath and Private Means of Access (PMA).

- 1.1.6. The Applicant is proposing eight changes to the Project in this Change Application. The locations of the Proposed Changes, situated within the western part of the Order limits in Schemes 01 - 05, are shown in Figure 1.
- 1.1.7. Proposed Changes 1 to 5 and Proposed Change 8 are located within Scheme 0405 – Temple Sowerby to Appleby; Proposed Change 6 is located within Scheme 0102 – M6 J40 to Kemplay Bank; and Proposed Change 7 is located within Scheme 03 – Penrith to Temple Sowerby as shown on Figure 1.

Figure 1 – Location of the Proposed Changes

1.1.8. The Proposed Changes are non-material changes for the following reasons:

- They will not result in any new or materially different likely significant effects on the environment;
- They do not require a Habitats Regulations Assessment or a new or additional protected species licence;
- They do not seek authorisation for the compulsory acquisition of any additional land or an interest in land or an interest in or rights over land beyond what is already authorised in the Order; and
- The likely impacts of the changes on local people and businesses are not considered to be sufficient to indicate that the changes should be considered as material.

1.1.9. The purpose of this Application Statement is to describe the proposals in this Non-Material Change Application; identify and assess the likely impacts of the proposals on businesses and residents in Section 2 and set out how pre-application engagement with stakeholders has contributed to the proposals in Section 4. The Application Statement also explains why the application is for a non-material change to the Order, with reference to the guidance on the assessment of materiality in the Department for Communities and Local Government's (DCLG) Guidance on Changes to Development Consent Orders (December 2015).

1.2. The context for the Proposed Changes

- 1.2.1. Since the Order was made by the Secretary of State for Transport in March 2024, the Applicant has continued to engage and negotiate with parties with an interest in land affected by the Project (including those with an interest in land which is subject to powers of compulsory acquisition, affected persons, and with other interested parties including Westmorland and Furness Council (formerly Cumbria County Council, and Eden District Council) in their capacity as Local Highway Authority and Local Planning Authority.
- 1.2.2. The Applicant has carried out consultation on the Proposed Changes as presented in Section 4. Full details of the eight Proposed Changes are set out in Section 2 of this Application Statement.
- 1.2.3. The Proposed Changes have arisen as a result of the process of detailed design, discussions with affected persons and interested parties, where opportunities have been identified to reduce the environmental impacts of the Project, the amount of land required and the complexity of construction, as well as incorporate further safety benefits, building on the assessment work carried out to date.
- 1.2.4. Most of the Proposed Changes take the Authorised Development outside of the Limits of Deviation (LoD) as defined in article 7 of the Order and accompanying Works Plans.
- 1.2.5. LoD are defined boundaries within which the Authorised Development as consented must be designed and constructed. They are commonly incorporated into Development Consent Orders (DCO) to allow large complex projects the flexibility to design around previously unidentified obstacles. The LoD set a limit within which specific parts of the Authorised Development can move laterally or vertically without requiring reassessment or an amendment to the Order.
- 1.2.6. Separate to the Proposed Changes sought within this Change Application, National Highways has implemented some minor changes to the Project which fall within the LoD. These minor changes are not shown on the drawings accompanying this Change Application. However, the Proposed Changes are designed alongside the minor changes and as a result there may be occasions where the Proposed Changes may appear disconnected on the drawings.
- 1.2.7. This approach follows advice received during the pre-application advice meetings with representatives of the Transport Infrastructure Planning Unit (TIPU) of Department for Transport.

2. The Proposed Non-Material Changes

2.1. General

- 2.1.1. The Proposed Changes, if approved, will require amendments to be made to various documents including the Order and its certified documents. Details of the changes proposed to the made Order and associated documents are set out in the Amendment Order and in the tracked changed version of the made Order that form part of this Change Application. A plan showing the locations of the Proposed Changes are shown above in Figure 1 of this Statement.
- 2.1.2. Annex B provides the full list of revised drawings accompanying this Change Application and the corresponding sheets within the certified drawing packs that they would replace.
- 2.1.3. It is not the intention of this Change Application to replace all drawings certified under Schedule 10 to the Order. Where the Proposed Changes do not alter an approved drawing sheet or entire drawing pack, revised drawings have not been submitted.
- 2.1.4. This approach reflects the discussion held with TIPU at the pre-application meeting held on 3 October 2024 and follow-up email correspondence dated 23 October 2024.
- 2.1.5. The subheadings below set out each of the eight Proposed Changes. The subheadings highlight what was authorised under the DCO for each relevant Proposed Change, a description of the change, a drawing showing the change overlaid with what was consented and a rationale for each change.

2.2. Proposed Change 1 - Cross Street

- 2.2.1. Cross Street is a local road (C3030) in Kirkby Thore, passing Kirkby Thore Primary School, then heading northwards towards Newbiggin. As the alignment of the new A66 will cut across Cross Street, the Order authorises the realignment of Cross Street and the provision of a new overbridge crossing the new A66.

The Authorised Development

- 2.2.2. Part 3 of Schedule 1 authorises the following relevant work numbers for this Proposed Change:
- The realignment of Priest Lane and its junction with the realigned Cross Street is authorised under Work No. 0405-6C;
 - Cross Street Bridge is authorised under Work No. 0405-7; and
 - The realignment of a Bridleway and its connection to Cross Street is authorised under Work No 0405-8.

2.2.3. The following plans certified under article 49 of the Order and listed in Schedule 10 thereto, define the alignment of Cross Street and connecting roads and private means of access:

- Engineering section drawings: cross sections, comprising the following:
 - Engineering Section Drawings (Cross Sections) Scheme 0405 Temple Sowerby to Appleby Sheet 1 of 8 HE565627-AMY-HAC-S0405-DR-CH-301101
- Engineering section drawings: plans and profiles, comprising the following:
 - Engineering Section Drawings (Plans and Profiles) Scheme 0405 Temple Sowerby to Appleby Sheet 1 of 24 HE565627-AMY-HAC-S0405-DR-CH-301001
 - Engineering Section Drawings (Plans and Profiles) Scheme 0405 Temple Sowerby to Appleby Sheet 2 of 24 HE565627-AMY-HAC-S0405-DR-CH-301002
 - Engineering Section Drawings (Plans and Profiles) Scheme 0405 Temple Sowerby to Appleby Sheet 8 of 24 HE565627-AMY-HAC-S0405-DR-CH-301008
 - Engineering Section Drawings (Plans and Profiles) Scheme 0405 Temple Sowerby to Appleby Sheet 9 of 24 HE565627-AMY-HAC-S0405-DR-CH-301009
 - Engineering Section Drawings (Plans and Profiles) Scheme 0405 Temple Sowerby to Appleby Sheet 17 of 24 HE565627-AMY-HAC-S0405-DR-CH-301017
 - Engineering Section Drawings (Plans and Profiles) Scheme 0405 Temple Sowerby to Appleby Sheet 18 of 24 HE565627-AMY-HAC-S0405-DR-CH-301018
 - Engineering Section Drawings (Plans and Profiles) Scheme 0405 Temple Sowerby to Appleby Sheet 19 of 24 HE565627-AMY-HAC-S0405-DR-CH-301019
- Rights of way and access plans, comprising the following:
 - Rights of Way and Access Plans Scheme 0405 Temple Sowerby to Appleby Sheet 1 of 7 HE565627-AMY-HAC-S0405-DR-CH-301401
 - Rights of Way and Access Plans Scheme 0405 Temple Sowerby to Appleby Sheet 2 of 7 HE565627-AMY-HAC-S0405-DR-CH-301402
- Works plans, comprising the following:
 - Works Plans Scheme 0405 Temple Sowerby to Appleby Sheet 2 of 7 HE565627-AMY-HAC-S0405-DR-CH-301202

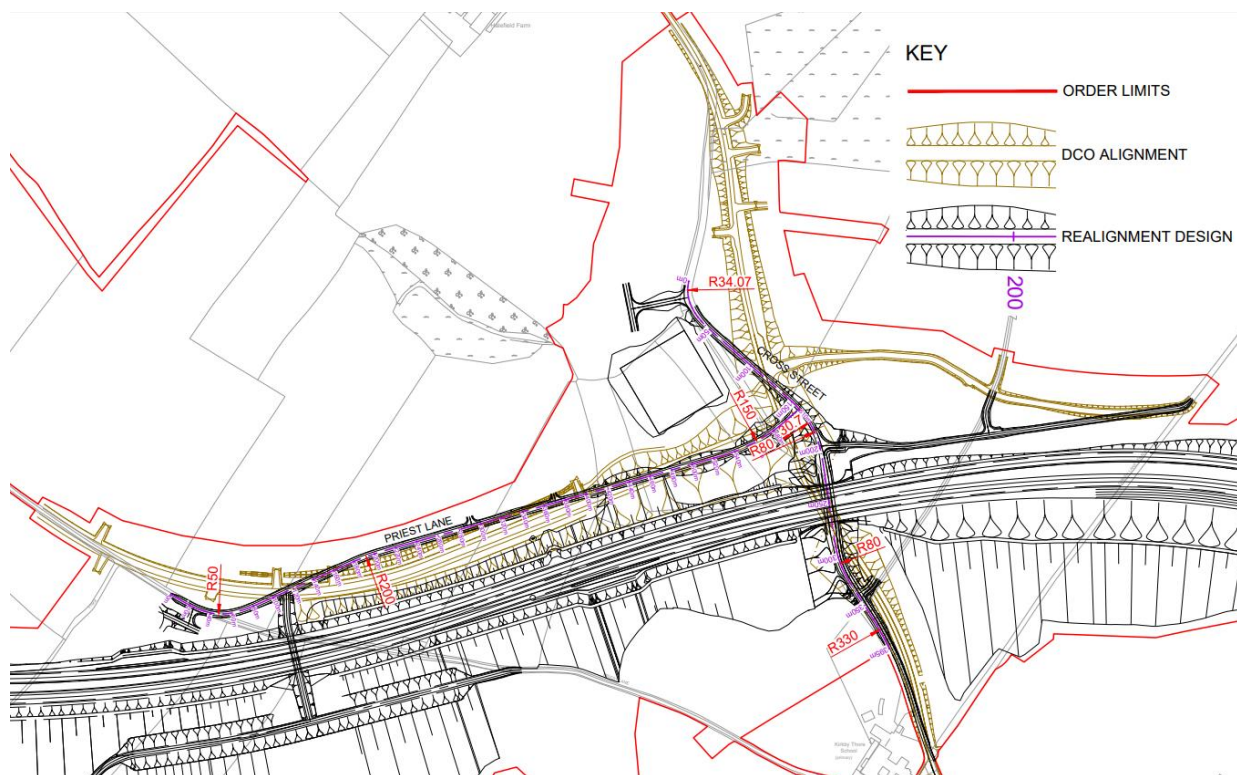
- Works Plans Scheme 0405 Temple Sowerby to Appleby Sheet 3 of 7 HE565627-AMY-HAC-S0405-DR-CH-301203
- Classification of roads plans, comprising the following:
 - Classification of Roads Plans Scheme 0405 Temple Sowerby to Appleby Sheet 1 of 2 HE565627-AMY-HAC-S0405-DR-CH-301301
 - Classification of Roads Plans Scheme 0405 Temple Sowerby to Appleby Sheet 2 of 2 HE565627-AMY-HAC-S0405-DR-CH-301302
- De-trunking plans, comprising the following:
 - De-Trunking Plans Scheme 0405 Temple Sowerby to Appleby Sheet 1 of 6 HE565627-AMY-HAC-S0405-DR-CH-301701
 - De-Trunking Plans Scheme 0405 Temple Sowerby to Appleby Sheet 2 of 6 HE565627-AMY-HAC-S0405-DR-CH-301702
- Traffic regulation measures: clearways and prohibitions, comprising the following:
 - Traffic Regulation Measures (Clearways and Prohibitions) Plans Scheme 0405 Temple Sowerby to Appleby Sheet 1 of 7 HE565627-AMY-HAC-S0405-DR-CH-301601
 - Traffic Regulation Measures (Clearways and Prohibitions) Plans Scheme 0405 Temple Sowerby to Appleby Sheet 2 of 7 HE565627-AMY-HAC-S0405-DR-CH-301602
- Traffic regulation measure: speed limits, comprising the following:
 - Traffic Regulation Measures (Speed Limits) Plans Scheme 0405 Temple Sowerby to Appleby Sheet 1 of 7 HE565627-AMY-HAC-S0405-DR-CH-301501
 - Traffic Regulation Measures (Speed Limits) Plans Scheme 0405 Temple Sowerby to Appleby Sheet 2 of 7 HE565627-AMY-HAC-S0405-DR-CH-301502

Description of Proposed Change

- 2.2.4. The Proposed Change comprises the realignment of the Cross Street overbridge, approaches and the connecting junctions and PMA. The lateral alignment of Cross Street would be adjusted by up to 30.7 metres beyond the current lateral LoD for the consented Work No. 0405-7. The LoD are currently 3 metres to the east of the centre of Cross Street and to the extent of the corresponding fine dashed green line as shown on the consented Works Plans to the west.

- 2.2.5. The Priest Lane junction with Cross Street would be relocated beyond the current lateral LoD for consented Work No.0405-6C which is 3 metres to the south of the centre of Priest Lane and to the extent of the corresponding fine dashed green line as shown on the consented Works Plans to the north.
- 2.2.6. The new Bridleway between the realigned Cross Street and Bridleway 336/018 would be relocated beyond the current LoD for the consented Work No. 0405-8 which is 3 metres to the north of the centre of Priest Lane and to the extent of the corresponding fine dashed green line as shown on the consented Works Plans to the south.
- 2.2.7. Proposed Change 1 (Cross Street) therefore comprises the realignment of the centreline for each of the consented Work Nos. 0405-7, 0405-6C and 0405-8. The current LoD (as stated above) would then apply to the realigned centrelines for those numbered works.

Figure 2 – Comparison showing the Proposed Change overlaid on the Authorised Development



Rationale for the Proposed Change

- 2.2.8. The Proposed Change is driven by engagement with Westmorland and Furness Council (WaFC), as both the Local Highway Authority (LHA) and Local Planning Authority (LPA), to improve road safety. Cross Street has been re-designed to agreed standards following consultations with WaFC. WaFC's agreement to the Proposed Change is evidenced in Annex C.
- 2.2.9. The curve of Cross Street on the northern approach to the overbridge is proposed to be tighter to encourage lower vehicle speeds on the approach to Kirkby Thore School.
- 2.2.10. The realignment also enables a reduction in the amount of third party land needed for the Project at this location as the realigned road would tie in earlier to the existing road. The landowners agreement with the Proposed Change is evidenced in Annex C1.

Changes to Certified Documents

- 2.2.11. Annex B lists the certified documents that must be updated as a result of this Proposed Change, together with the revised drawing sheets accompanying this application, which would replace the corresponding certified versions.

Amendments to DCO Schedule 1 and 2

- 2.2.12. Work No. 0405-6C in Schedule 1 sub-paragraphs (iii) and (iv) has been amended as a result of this Proposed Change.
- 2.2.13. It is proposed that Schedule 2 to the DCO is amended in order to reflect the Proposed Change. Table 1, below, provides a summary of these proposed amendments. A draft Amendment Order has been submitted as part of this application.
- 2.2.14. Schedule 2 details the permanent stopping up of highways and private means of access and the provision of new highways and private means of access. The provisions described in the Schedule correspond to and are shown on the Rights of Way and Access Plans.
- 2.2.15. Part 1 of Schedule 2 sets out the highways to be stopped up for which a substitute is to be provided, and new highways which are otherwise to be provided. Part 3 of Schedule 2 sets out the PMAs to be stopped up, for which a substitute is to be provided, and new PMAs which are otherwise to be provided.

Table 1 – Proposed Change 1 Amendments to DCO Schedule 2

Column (2) <i>'Highway to be stopped up'</i>	Summary of changes to Column (3) <i>'Extent of stopping up'</i>	Summary of changes to Column (4) <i>'New highway to be substituted/provided'</i>
Scheme 0405 – Temple Sowerby to Appleby – Rights of Way and Access Plans Sheets 1 and 2 – Schedule 2 Part 1		

Column (2) <i>'Highway to be stopped up'</i>	Summary of changes to Column (3) <i>'Extent of stopping up'</i>	Summary of changes to Column (4) <i>'New highway to be substituted/provided'</i>
Priest Lane	The length of highway to be stopped up has changed.	Realigned Priest Lane: The distance of the realigned Priest Lane from the junction of the existing Cross Street, and the length of the realigned Priest Lane, have been changed.
Cross Street		Realigned Cross Street: The length of the realigned Cross Street has been changed.
Bridleway 336/018	The length of the highway to be stopped up has changed.	Reference B: The length of the substitute new bridleway and the length to tie into the existing Bridleway 336/018 have changed.
—	—	Reference K: The length of the new footpath between existing Bridleway 336/018 and existing Footpath 336/017 has changed.
—	—	Reference D*: The length of the new footway on the realigned Cross Street has changed.

Column (2) <i>'Private means of access to be stopped up'</i>	Summary of changes to (3) <i>Extent of stopping up'</i>	Summary of changes to (4) <i>New private means of access to be substituted/provided'</i>
Scheme 0405 – Temple Sowerby to Appleby – Rights of Way and Access Plans Sheets 1 and 2 – Schedule 2 Part 3		
Reference d: No change.	No change.	Reference 8: PMA has moved adjacent to Bridleway 336/007.
—	—	Reference 10: The distance and direction from the junction of the existing Priest Lane have changed.
Reference e: No Change.	No change.	Reference 11: The distance from the junction of the existing Priest Lane has changed.
Reference i: No change.	No change.	Reference 12: The description of the location of the new private means of access has been removed.
—	—	Reference 13: This has been removed.

Column (2) 'Private means of access to be stopped up'	Summary of changes to '(3) Extent of stopping up'	Summary of changes to '(4) New private means of access to be substituted/provided'
—	—	Reference 14: This has been removed.
—	—	Reference 15: This has been removed.
—	—	Reference 16: The drainage 'pond' is now a 'tank', and the distance from the proposed Cross Street Bridge has changed.
Reference l: This has been removed.	This has been removed.	Reference 17: This has been removed.
Reference k: This has been removed.	This has been removed.	Reference 18: This has been removed.
—	—	Reference 19: This has been removed.
Reference p: No change.	The description and length of the stopping up along Bridleway 336/018 has been updated.	References 20 and 78: No change.
—	—	Reference 21: This has been removed.
—	—	Reference 22: This has been removed.

2.3. Proposed Change 2 - Green Lane

- 2.3.1. Green Lane comprises part of Footpath 336/017 and is also a private means of access to land north of Kirkby Thore. The Authorised Development involves the realignment of Green Lane to the west of its existing alignment and provision of a new overbridge crossing the new A66 and a reconnection to the existing alignment of Green Lane to the south. The realignment of Green Lane as consented also provides for a replacement private means of access and a diversion of the existing public footpath.

The Authorised Development

- 2.3.2. The Green Lane Bridge, subject to this Proposed Change, is authorised under Work No. 0405-9 in Part 3 of Schedule 1 of the Order.
- 2.3.3. The following plans certified under article 49 of the Order and listed in Schedule 10 thereto, define the alignment of Green Lane and connecting junctions and private means of access:

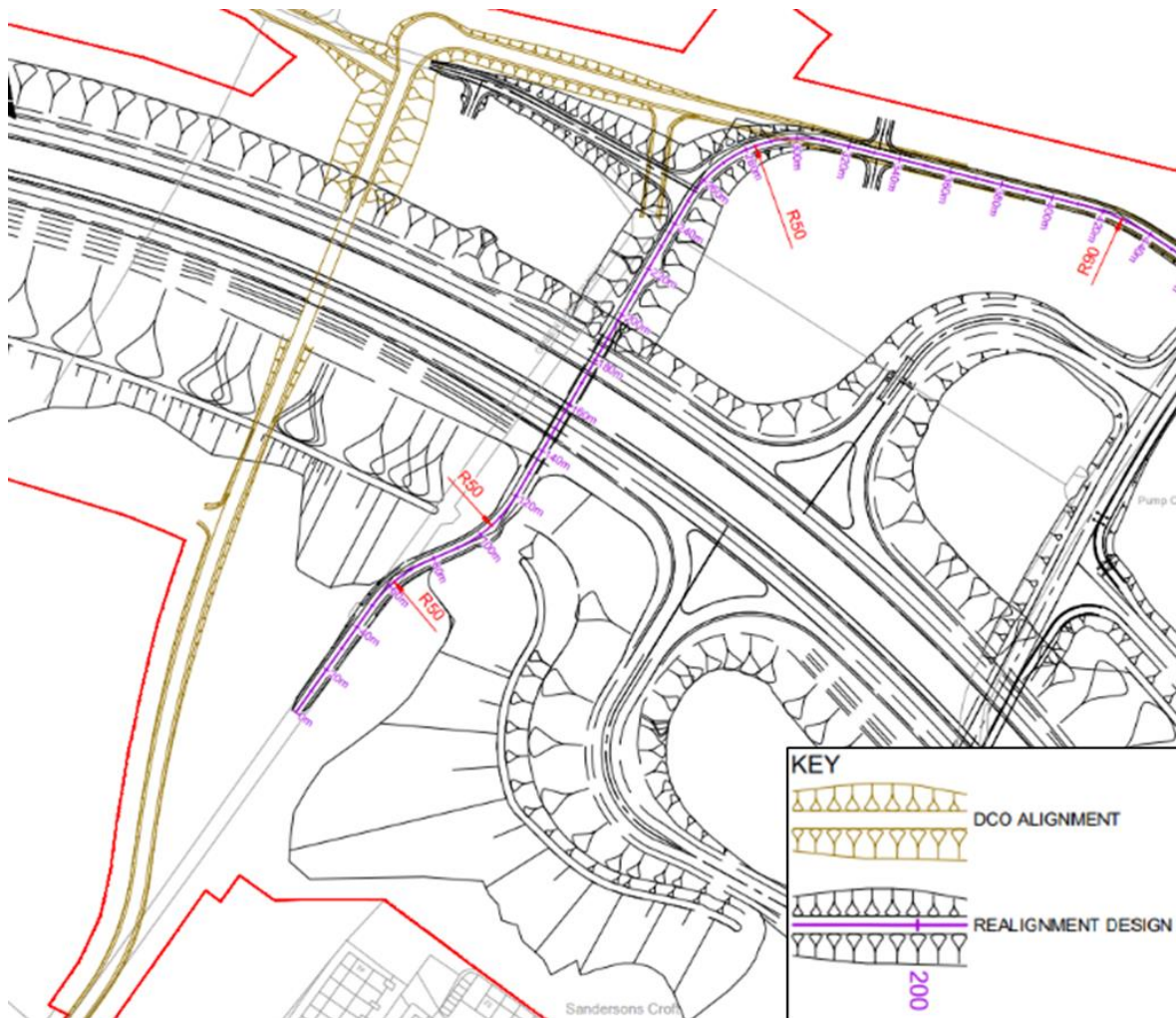
- Engineering section drawings: plans and profiles, comprising the following:
 - Engineering Section Drawings (Plans and Profiles) Scheme 0405 Temple Sowerby to Appleby Sheet 2 of 24 HE565627-AMY-HAC-S0405-DR-CH-301002
 - Engineering Section Drawings (Plans and Profiles) Scheme 0405 Temple Sowerby to Appleby Sheet 9 of 24 HE565627-AMY-HAC-S0405-DR-CH-301009
 - Engineering Section Drawings (Plans and Profiles) Scheme 0405 Temple Sowerby to Appleby Sheet 17 of 24 HE565627-AMY-HAC-S0405-DR-CH-301017
 - Engineering Section Drawings (Plans and Profiles) Scheme 0405 Temple Sowerby to Appleby Sheet 19 of 24 HE565627-AMY-HAC-S0405-DR-CH-301019
 - Engineering Section Drawings (Plans and Profiles) Scheme 0405 Temple Sowerby to Appleby Sheet 20 of 24 HE565627-AMY-HAC-S0405-DR-CH-301020
- Rights of way and access plans, comprising the following:
 - Rights of Way and Access Plans Scheme 0405 Temple Sowerby to Appleby Sheet 2 of 7 HE565627-AMY-HAC-S0405-DR-CH-301402
 - Rights of Way and Access Plans Scheme 0405 Temple Sowerby to Appleby Sheet 3 of 7 HE565627-AMY-HAC-S0405-DR-CH-301403
- Works plans, comprising:
 - Works Plans Scheme 0405 Temple Sowerby to Appleby Sheet 2 of 7 HE565627-AMY-HAC-S0405-DR-CH-301202
 - Works Plans Scheme 0405 Temple Sowerby to Appleby Sheet 3 of 7 HE565627-AMY-HAC-S0405-DR-CH-301203
- Classification of roads plans, comprising the following:
 - Classification of Roads Plans Scheme 0405 Temple Sowerby to Appleby Sheet 1 of 2 HE565627-AMY-HAC-S0405-DR-CH-301301
 - Classification of Roads Plans Scheme 0405 Temple Sowerby to Appleby Sheet 2 of 2 HE565627-AMY-HAC-S0405-DR-CH-301302
- De-trunking plans, comprising the following:
 - De-Trunking Plans Scheme 0405 Temple Sowerby to Appleby Sheet 2 of 6 HE565627-AMY-HAC-S0405-DR-CH-301702

- Traffic regulation measures: clearways and prohibitions, comprising the following:
 - Traffic Regulation Measures (Clearways and Prohibitions) Plans Scheme 0405 Temple Sowerby to Appleby Sheet 2 of 7 HE565627-AMY-HAC-S0405-DR-CH-301602
 - Traffic Regulation Measures (Clearways and Prohibitions) Plans Scheme 0405 Temple Sowerby to Appleby Sheet 3 of 7 HE565627-AMY-HAC-S0405-DR-CH-301603
- Traffic regulation measure: speed limits, comprising the following:
 - Traffic Regulation Measures (Speed Limits) Plans Scheme 0405 Temple Sowerby to Appleby Sheet 2 of 7 HE565627-AMY-HAC-S0405-DR-CH-301502
 - Traffic Regulation Measures (Speed Limits) Plans Scheme 0405 Temple Sowerby to Appleby Sheet 3 of 7 HE565627-AMY-HAC-S0405-DR-CH-301503

Description of Proposed Change

- 2.3.4. The Proposed Change comprises keeping the current alignment of Green Lane and constructing the new Green Lane overbridge on the existing alignment of Green Lane. The realignment compared to the work currently authorised by the Order would be beyond the current lateral LoD of the authorised Work No. 0405-9 which are 3 metres either side of the centreline.
- 2.3.5. Proposed Change 2 (Green Lane) therefore comprises the realignment of the centreline of the authorised Work No. 0405-9. The current LoD (as stated above) would then apply to the realigned centreline for this numbered work.

Figure 3 – Comparison showing the Proposed Change to the alignment of Green Lane overlaid on the Authorised Development



Rationale for the Proposed Change

- 2.3.6. The Proposed Change was led by the Applicant but was developed following engagement with the landowner, whose agreement with the Proposed Change is evidenced in Annex C1.
- 2.3.7. Reverting the alignment to the existing alignment of Green Lane would reduce the amount of third party land required and avoid severing the agricultural field west of the current Green Lane alignment. The bridge would also span a shorter distance, thus requiring less earthworks.

Changes to Certified Documents

- 2.3.8. Annex B lists the certified documents that must be updated as a result of this Proposed Change, together with the revised drawing sheets accompanying this application which would replace the corresponding certified versions.

Amendments to DCO Schedule 1 and 2

- 2.3.9. Work No. 0405-9 in Schedule 1 sub-paragraph (a) has been amended as a result of this Proposed Change.
- 2.3.10. It is proposed that Schedule 2 to the DCO is amended in order to reflect the Proposed Change. Table 2, below, provides a summary of these proposed amendments. A draft Amendment Order has been submitted as part of this application.
- 2.3.11. Schedule 2 details the permanent stopping up of highways and private means of access and the provision of new highways and private means of access. The provisions described in the Schedule correspond to and are shown on the Rights of Way and Access Plans.
- 2.3.12. Part 3 of Schedule 2 sets out the PMAs to be stopped up, for which a substitute is to be provided, and new PMAs which are otherwise to be provided.

Table 2 – Proposed Change 2 Amendments to DCO Schedule 2

Column (2) <i>'Private means of access to be stopped up'</i>	Summary of changes to <i>'(3) Extent of stopping up'</i>	Summary of changes to <i>'(4) New private means of access to be substituted/provided'</i>
Scheme 0405 – Temple Sowerby to Appleby – Right of Way and Access Plans Sheets 2 and 3 – Schedule 2 Part 3		
Reference q: and Reference r (Green Lane Track): Reference r (Green Lane Track) has been removed.	No change.	Reference 23: This has been removed. This has been substituted by References 24 and 26 with relevant amendments having been made to these references in the Schedule.
Reference s: No change.	The length and the location of the stopping up of Green Lane Track have changed.	To be substituted by Reference 24, a new private means of access, and Reference 26, another new private means of access.

2.4. Proposed Change 3 - Long Marton

Long Marton is a local road (C3004), south-west of Long Marton village. It connects the village with the existing A66. The Authorised Development involves the realignment of Long Marton to the south of its existing alignment and a new underpass beneath the new A66.

The Authorised Development

- 2.4.1. The realignment of Long Marton which is subject to this Proposed Change is authorised under Work No. 0405-16 of Part 3 of Schedule 1 of the Order.

2.4.2. The following plans certified under article 49 of the Order and listed in Schedule 10 thereto, define the alignment of Long Marton and connecting junctions and private means of access:

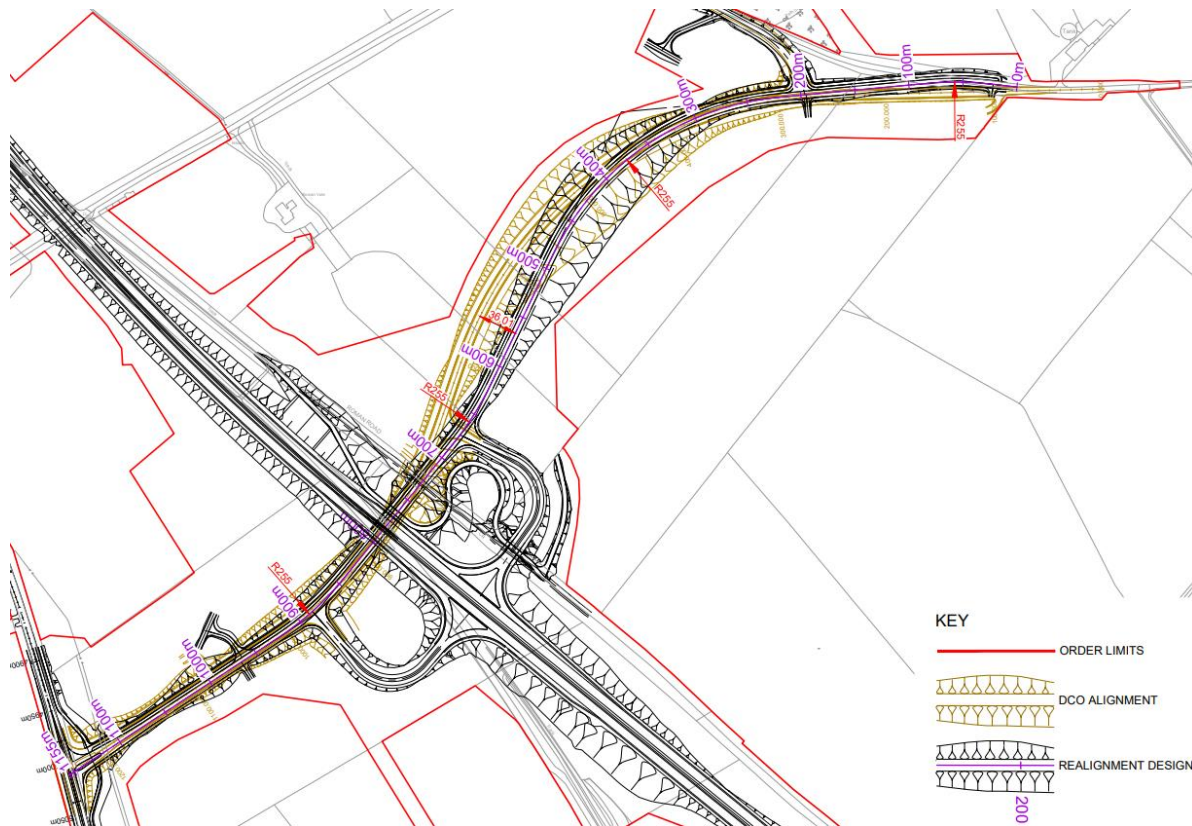
- Engineering section drawings: cross sections, comprising the following:
 - Engineering Section Drawings (Cross Sections) Scheme 0405 Temple Sowerby to Appleby Sheet 5 of 8 HE565627-AMY-HAC-S0405-DR-CH-301105
- Engineering section drawings: plans and profiles, comprising the following:
 - Engineering Section Drawings (Plans and Profiles) Scheme 0405 Temple Sowerby to Appleby Sheet 4 of 24 HE565627-AMY-HAC-S0405-DR-CH-301004
 - Engineering Section Drawings (Plans and Profiles) Scheme 0405 Temple Sowerby to Appleby Sheet 5 of 24 HE565627-AMY-HAC-S0405-DR-CH-301005
 - Engineering Section Drawings (Plans and Profiles) Scheme 0405 Temple Sowerby to Appleby Sheet 11 of 24 HE565627-AMY-HAC-S0405-DR-CH-301011
 - Engineering Section Drawings (Plans and Profiles) Scheme 0405 Temple Sowerby to Appleby Sheet 12 of 24 HE565627-AMY-HAC-S0405-DR-CH-301012
 - Engineering Section Drawings (Plans and Profiles) Scheme 0405 Temple Sowerby to Appleby Sheet 22 of 24 HE565627-AMY-HAC-S0405-DR-CH-301022
 - Engineering Section Drawings (Plans and Profiles) Scheme 0405 Temple Sowerby to Appleby Sheet 23 of 24 HE565627-AMY-HAC-S0405-DR-CH-301023
- Rights of way and access plans, comprising the following:
 - Rights of Way and Access Plans Scheme 0405 Temple Sowerby to Appleby Sheet 5 of 7 HE565627-AMY-HAC-S0405-DR-CH-301405
- Works plans, comprising the following:
 - Works Plans Scheme 0405 Temple Sowerby to Appleby Sheet 5 of 7 HE565627-AMY-HAC-S0405-DR-CH-301205
- Classification of roads plans, comprising the following:
 - Classification of Roads Plans Scheme 0405 Temple Sowerby to Appleby Sheet 1 of 2 HE565627-AMY-HAC-S0405-DR-CH-301301

- Classification of Roads Plans Scheme 0405 Temple Sowerby to Appleby Sheet 2 of 2 HE565627-AMY-HAC-S0405-DR-CH-301302
- De-trunking plans, comprising the following:
 - De-Trunking Plans Scheme 0405 Temple Sowerby to Appleby Sheet 4 of 6 HE565627-AMY-HAC-S0405-DR-CH-301704
- Traffic regulation measures: clearways and prohibitions, comprising the following:
 - Traffic Regulation Measures (Clearways and Prohibitions) Plans Scheme 0405 Temple Sowerby to Appleby Sheet 5 of 7 HE565627-AMY-HAC-S0405-DR-CH-301605
- Traffic regulation measure: speed limits, comprising the following:
 - Traffic Regulation Measures (Speed Limits) Plans Scheme 0405 Temple Sowerby to Appleby Sheet 5 of 7 HE565627-AMY-HAC-S0405-DR-CH-301505

Description of Proposed Change

- 2.4.3. The Proposed Change comprises the realignment of Long Marton to reduce the span of the bridge, which would also reduce the amount of third party land required for its delivery. This realignment would also necessitate a reduction in the design speed to 85kph (50mph) which would be undertaken through a Traffic Regulation Order (TRO) in accordance with article 42 of the Order, separate to this Change Application.
- 2.4.4. The realigned design falls beyond the current lateral LoD of the authorised Work No. 0405-16 which are 3 metres either side of the centreline, the centre of the approved DCO road alignment.
- 2.4.5. Proposed Change 3 (Long Marton) therefore comprises the realignment of the centreline of Work No. 0405-16. The current LoD (as stated above) would then apply to the realigned centreline for this numbered work.

Figure 4 – Comparison showing the Proposed Change overlaid on the Authorised Development



Rationale for the Proposed Change

- 2.4.6. The Proposed Change is driven by engagement with Westmorland and Furness Council (WaFC), as both the LHA and LPA, to improve road safety. Long Marton Road has been re-designed to agreed standards following consultations with WaFC. The realignment of the bridge and approaches would provide a geometry that would encourage lower vehicle speeds. WaFC's agreement to the Proposed Change is evidenced in Annex C3.
- 2.4.7. The realignment would also reduce the length of the underpass, resulting in a minor reduction in carbon emissions through reducing the amount of materials required.

Changes to Certified Documents

- 2.4.8. Annex B lists the certified documents that must be updated as a result of this Proposed Change, together with the revised drawing sheets accompanying this application, which would replace the corresponding certified versions.

Amendments to DCO Schedule 2

- 2.4.9. It is proposed that Schedule 2 to the DCO is amended in order to reflect the Proposed Change. Table 3, below, provides a summary of these proposed amendments. A draft Amendment Order is submitted with this application. There are no changes required to Schedule 1.
- 2.4.10. Schedule 2 details the permanent stopping up of highways and private means of access and the provision of new highways and private means of access. The provisions described in the Schedule correspond to and are shown on the Rights of Way and Access Plans.
- 2.4.11. Part 1 of Schedule 2 is the highways to be stopped up for which a substitute is to be provided, and new highways which are otherwise to be provided. Part 3 of Schedule 2 is the PMAs to be stopped up, for which a substitute is to be provided, and new PMAs which are otherwise to be provided.

Table 3 – Proposed Change 3 Amendments to DCO Schedule 2

Column (2) <i>'Highway to be stopped up'</i>	Summary of changes to '(3) <i>Extent of stopping up</i> '	Summary of changes to '(4) <i>New highway to be substituted/provided</i> '
Scheme 0405 – Temple Sowerby to Appleby – Rights of Way and Access Plan Sheet 5 – Schedule 2 Part 1		
Long Marton (Road): No change.	The length of the extent of stopping up has changed.	To be substituted by:- No change.
–	–	Reference H*: The length of the new cycleway has changed.

Column (2) <i>'Private means of access to be stopped up'</i>	Summary of changes to '(3) <i>Extent of stopping up</i> '	Summary of changes to '(4) <i>New private means of access to be substituted/provided</i> '
Scheme 0405 – Temple Sowerby to Appleby – Rights of Way and Access Plans Sheets 4 and 5 – Schedule 2 Part 3		
–	–	Reference 50: The length of the new private means of access to be provided has changed.
–	–	Reference 56: This has been removed.
–	–	Reference 53: The maintenance strip has been removed, and the distance from the junction of the existing Long Marton (Road) and realigned Long Marton has changed.

Column (2) 'Private means of access to be stopped up'	Summary of changes to '(3) Extent of stopping up'	Summary of changes to '(4) New private means of access to be substituted/provided'
—	—	Reference 54: The maintenance strip has been removed.
—	—	Reference 58: The maintenance strip has been removed.
—	—	Reference 59: The location of this new private means of access has changed.

2.5. Proposed Change 4 - Access track at Kirkby Thore

- 2.5.1. The Order authorises the formation of a new access track providing a private means of access and a footpath connecting the realigned Cross Street with the realigned Green Lane on an alignment located to the north of Kirkby Thore and to the south of the new A66.

The Authorised Development

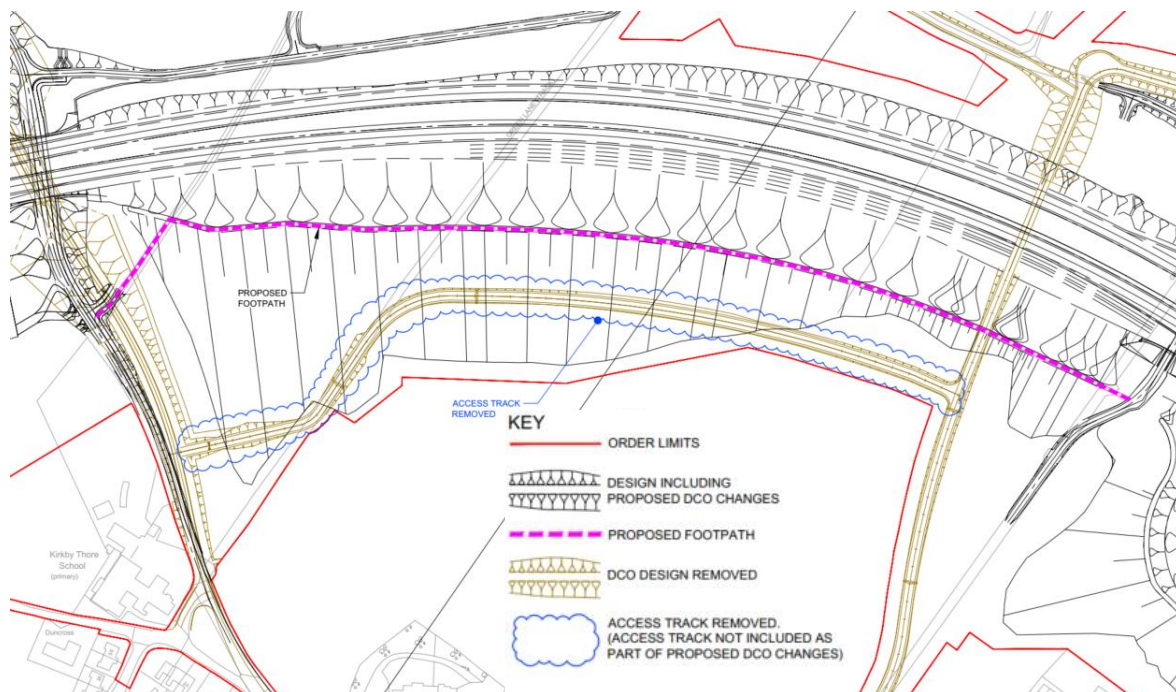
- 2.5.2. The provision of a new access track, which is subject to this Proposed Change is authorised under Work No. 0405-9 in Part 3 of Schedule 1 of the Order.
- 2.5.3. The following plans certified under article 49 of the Order and listed in Schedule 10 thereto, define the alignment of farm access track and connecting junctions and private means of access:
- The Engineering section drawings: cross sections, comprising the following:
 - Engineering Section Drawings (Cross Sections) Scheme 0405 Temple Sowerby to Appleby Sheet 3 of 8 HE565627-AMY-HAC-S0405-DR-CH-301110
 - Engineering section drawings: plans and profiles, comprising the following:
 - Engineering Section Drawings (Plans and Profiles) Scheme 0405 Temple Sowerby to Appleby Sheet 2 of 24 HE565627-AMY-HAC-S0405-DR-CH-301002
 - Engineering Section Drawings (Plans and Profiles) Scheme 0405 Temple Sowerby to Appleby Sheet 9 of 24 HE565627-AMY-HAC-S0405-DR-CH-301009
 - Engineering Section Drawings (Plans and Profiles) Scheme 0405 Temple Sowerby to Appleby Sheet 17 of 24 HE565627-AMY-HAC-S0405-DR-CH-301017

- Engineering Section Drawings (Plans and Profiles) Scheme 0405 Temple Sowerby to Appleby Sheet 19 of 24 HE565627-AMY-HAC-S0405-DR-CH-301019
- Rights of way and access plans, comprising the following:
 - Rights of Way and Access Plans Scheme 0405 Temple Sowerby to Appleby Sheet 2 of 7 HE565627-AMY-HAC-S0405-DR-CH-301402
 - Rights of Way and Access Plans Scheme 0405 Temple Sowerby to Appleby Sheet 3 of 7 HE565627-AMY-HAC-S0405-DR-CH-301403
- Works plans, comprising the following:
 - Works Plans Scheme 0405 Temple Sowerby to Appleby Sheet 2 of 7 HE565627-AMY-HAC-S0405-DR-CH-301202
 - Works Plans Scheme 0405 Temple Sowerby to Appleby Sheet 3 of 7 HE565627-AMY-HAC-S0405-DR-CH-301203
- Classification of roads plans, comprising the following:
 - Classification of Roads Plans Scheme 0405 Temple Sowerby to Appleby Sheet 1 of 2 HE565627-AMY-HAC-S0405-DR-CH-301301
 - Classification of Roads Plans Scheme 0405 Temple Sowerby to Appleby Sheet 2 of 2 HE565627-AMY-HAC-S0405-DR-CH-301302
- De-trunking plans, comprising the following:
 - De-Trunking Plans Scheme 0405 Temple Sowerby to Appleby Sheet 2 of 6 HE565627-AMY-HAC-S0405-DR-CH-301702
- Traffic regulation measures: clearways and prohibitions, comprising the following:
 - Traffic Regulation Measures (Clearways and Prohibitions) Plans Scheme 0405 Temple Sowerby to Appleby Sheet 2 of 7 HE565627-AMY-HAC-S0405-DR-CH-301602
 - Traffic Regulation Measures (Clearways and Prohibitions) Plans Scheme 0405 Temple Sowerby to Appleby Sheet 3 of 7 HE565627-AMY-HAC-S0405-DR-CH-301603
- Traffic regulation measure: speed limits, comprising the following:
 - Traffic Regulation Measures (Speed Limits) Plans Scheme 0405 Temple Sowerby to Appleby Sheet 2 of 7 HE565627-AMY-HAC-S0405-DR-CH-301502
 - Traffic Regulation Measures (Speed Limits) Plans Scheme 0405 Temple Sowerby to Appleby Sheet 3 of 7 HE565627-AMY-HAC-S0405-DR-CH-301503

Description of Proposed Change

- 2.5.4. The Proposed Change comprises the removal (i.e. not constructing) of the approved access track between the realigned Cross Street and realigned Green Lane, and instead creating a new section of PRow (as shown by the pink dashed line in Figure 5 below) along the bottom of the southern embankment of the new A66 between the realigned Cross Street and Green Lane.
- 2.5.5. Proposed Change 4 (Access Track at Kirkby Thore) therefore requires the change of a provision for a public right of way and private means of access between Green Lane and Cross Street to a PRow to be shown on an updated version of Sheet 2 of the Rights of Way and Access Plans.

Figure 5 – Comparison showing the Proposed Change overlaid on the Authorised Development



Rationale for the Proposed Change

- 2.5.6. During consultation with the landowner, they expressed that existing field access is sufficient without the need for the section of access track authorised by the DCO. The removal of the access track would reduce the severance of the farmland and reduce the amount of third party land required. The proposed new PRow along the bottom of the embankment ensures there is still a right of way between Cross Street and Green Lane following the proposed removal of the approved access track. The landowners agreement with the Proposed Change is evidenced in Annex C1.

Changes to Certified Documents

- 2.5.7. Annex B lists the certified documents that must be updated as a result of this Proposed Change, together with the proposed amended drawings, which would replace the corresponding certified version.

Amendments to DCO Schedule 2

- 2.5.8. Schedule 2 to the DCO is proposed to be amended in order to reflect the Proposed Change. Table 4, below, provides a summary of these proposed amendments. A draft Amendment Order is submitted as part of this application. There are no changes required to Schedule 1.
- 2.5.9. Schedule 2 details the permanent stopping up of highways and PMAs and the provision of new highways and PMAs. The provisions described in the Schedule correspond to and are shown on the Rights of Way and Access Plans.
- 2.5.10. Part 1 of Schedule 2 is highways to be stopped up for which a substitute is to be provided, and new highways which are otherwise to be provided. Part 3 of Schedule 2 is the PMAs to be stopped up, for which a substitute is to be provided, and new PMAs which are otherwise to be provided.

Table 4 – Proposed Change 4 Amendments to DCO Schedule 2

Column (2) <i>'Highway to be stopped up'</i>	Summary of changes to <i>'(3) Extent of stopping up'</i>	Summary of changes to <i>'(4) New highway to be substituted/provided'</i>
Scheme 0405 – Temple Sowerby to Appleby – Rights of Way and Access Plans Sheets 2 and 3 – Schedule 2 Part 1		
Footpath 336/017	The length of Footpath 336/017 to be stopped up has changed.	Reference C: The new footpath's direction and distance from the junction of Bridleway 336/018 with the existing Cross Street, as well as the direction and length of the new footpath, have changed.

Column (2) <i>'Private means of access to be stopped up'</i>	Summary of changes to <i>'(3) Extent of stopping up'</i>	Summary of changes to <i>'(4) New private means of access to be substituted/provided'</i>
Scheme 0405 – Temple Sowerby to Appleby – Rights of Way and Access Plans Sheets 2 and 3 – Schedule 2 Part 3		
–	–	Reference 22: This has been removed.
Reference q: and Reference r (Green Lande Track):	This has been removed.	Reference 23: This has been removed. This has been substituted by References 24 and 26 with relevant amendments having been

<i>Column (2) 'Private means of access to be stopped up'</i>	<i>Summary of changes to '(3) Extent of stopping up'</i>	<i>Summary of changes to '(4) New private means of access to be substituted/provided'</i>
Reference q has been removed.		made to these references in the Schedule.

2.6. Proposed Change 5 – B6542 and Spitals Farm Access

- 2.6.1. To the south east of Spitals Farm, the existing A66 will be de-trunked and re-classified as the new B6542, which, in addition, will comprise a length of new highway (also to be classified as the B6542), running largely adjacent to the new A66. The Order authorises the stopping up and replacement of a PMA to Spitals Farm, via an underpass beneath the new A66 and the re-classified as the B6542.

The Authorised Development

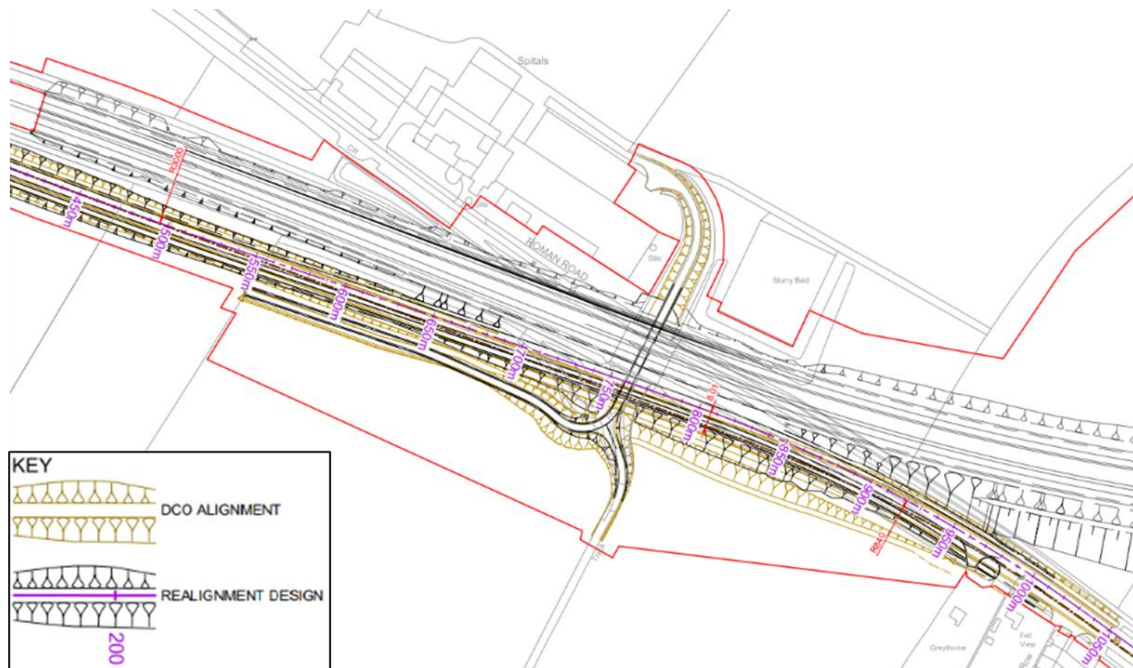
- 2.6.2. The realignment of Spitals Farm access which is subject to this Proposed Change is authorised under Work No. 0405-4A in Part 3 of Schedule 1 of the Order.
- 2.6.3. The following plans certified under article 49 of the Order and listed in Schedule 10 thereto, define the alignment of Spitals Farm access:
- Engineering section drawings: cross sections, comprising the following:
 - Engineering Section Drawings (Cross Sections) Scheme 0405 Temple Sowerby to Appleby Sheet 1 of 8 HE565627-AMY-HAC-S0405-DR-CH-301101
 - Engineering section drawings: plans and profiles, comprising the following:
 - Engineering Section Drawings (Plans and Profiles) Scheme 0405 Temple Sowerby to Appleby Sheet 1 of 24 HE565627-AMY-HAC-S0405-DR-CH-301001
 - Engineering Section Drawings (Plans and Profiles) Scheme 0405 Temple Sowerby to Appleby Sheet 8 of 24 HE565627-AMY-HAC-S0405-DR-CH-301008
 - Engineering Section Drawings (Plans and Profiles) Scheme 0405 Temple Sowerby to Appleby Sheet 15 of 24 HE565627-AMY-HAC-S0405-DR-CH-301015
 - Engineering Section Drawings (Plans and Profiles) Scheme 0405 Temple Sowerby to Appleby Sheet 16 of 24 HE565627-AMY-HAC-S0405-DR-CH-301016

- Rights of way and access plans, comprising the following:
 - Rights of Way and Access Plans Scheme 0405 Temple Sowerby to Appleby Sheet 1 of 7 HE565627-AMY-HAC-S0405-DR-CH-301401
- Works plans, comprising the following:
 - Works Plans Scheme 0405 Temple Sowerby to Appleby Sheet 1 of 7 HE565627-AMY-HAC-S0405-DR-CH-301201
- Classification of roads plans, comprising the following:
 - Classification of Roads Plans Scheme 0405 Temple Sowerby to Appleby Sheet 1 of 2 HE565627-AMY-HAC-S0405-DR-CH-301301
 - Classification of Roads Plans Scheme 0405 Temple Sowerby to Appleby Sheet 2 of 2 HE565627-AMY-HAC-S0405-DR-CH-301302
- De-trunking plans, comprising the following:
 - De-Trunking Plans Scheme 0405 Temple Sowerby to Appleby Sheet 1 of 6 HE565627-AMY-HAC-S0405-DR-CH-301701
- Traffic regulation measures: clearways and prohibitions, comprising the following:
 - Traffic Regulation Measures (Clearways and Prohibitions) Plans Scheme 0405 Temple Sowerby to Appleby Sheet 1 of 7 HE565627-AMY-HAC-S0405-DR-CH-301601
- Traffic regulation measure: speed limits, comprising the following:
 - Traffic Regulation Measures (Speed Limits) Plans Scheme 0405 Temple Sowerby to Appleby Sheet 1 of 7 HE565627-AMY-HAC-S0405-DR-CH-301501

Description of Proposed Change

- 2.6.4. The Proposed Change is to the alignment of the new B6542, moving up to 8 metres closer to the new A66. This exceeds the lateral LoD for the authorised Work No. 0405-4A which is 3 metres either side of the centreline of the road approved in the DCO.
- 2.6.5. Proposed Change 5 (B6542 and Spitals Farm access) therefore comprises the realignment of the centreline for the authorised Work No. 0405-4A. The current LoD (as stated above) would then apply to the realigned centreline for that numbered work. This change would also involve a change to the replacement private means of access to Spitals Farm, insofar as the width of the underpass (beneath the de-trunked A66 / new B6542 and the new A66) would be reduced.

Figure 6 – Comparison showing the Proposed Change overlaid on the Authorised Development



Rationale for the Proposed Change

- 2.6.6. The Proposed Change has arisen during design optimisation where an opportunity to reduce the length of the underpass has been identified. This would reduce the amount of associated earthworks and third party land required. It would also reduce the duration of construction of the side road and underpass and therefore reduce disruption to the public during the construction.
- 2.6.7. The landowner was consulted during the development of the design and has confirmed their agreement to the Proposed Change, which is evidenced in Annex C4.

Changes to Certified Documents

- 2.6.8. Annex B lists the certified documents that must be updated as a result of this Proposed Change, together with the proposed amended drawings, which would replace the corresponding certified version.

Amendments to DCO Schedule 2

- 2.6.9. Schedule 2 to the DCO is proposed to be amended in order to reflect the Proposed Change. Table 5, below, provides a summary of these proposed amendments. A draft Amendment Order is submitted with this application. There are no changes required to Schedule 1.
- 2.6.10. Schedule 2 details the permanent stopping up of highways and private means of access and the provision of new highways and private means of access. The provisions described in the Schedule correspond to and are shown on the Rights of Way and Access Plans.

2.6.11. Part 3 of Schedule 2 is the PMAs to be stopped up, for which a substitute is to be provided, and new PMAs which are otherwise to be provided.

Table 5 – Proposed Change 5 Amendments to DCO Schedule 2

Column (2) <i>'Private means of access to be stopped up'</i>	Summary of changes to <i>'(3) Extent of stopping up'</i>	Summary of changes to <i>'(4) New private means of access to be substituted/provided'</i>
Scheme 0405 – Temple Sowerby to Appleby – Rights of Way and Access Plan Sheet 1 – Schedule 2 Part 3		
Reference b: No change.	No change.	Reference 4: The distance of the new private means of access from the existing Spitals Farm Underpass and the length of the new private means of access have changed.

2.7. Proposed Change 6 - Kemplay Bank

2.7.1. The Authorised Development provides a new section of A66 to be constructed to pass beneath the new Kemplay Bank junction arrangement. This will tie into the existing A66 alignment.

The Authorised Development

2.7.2. The changes to the Kemplay Bank LOD which are subject to this Proposed Change are authorised under Work No. 0102-1D in Part 1 of Schedule 1 of the Order.

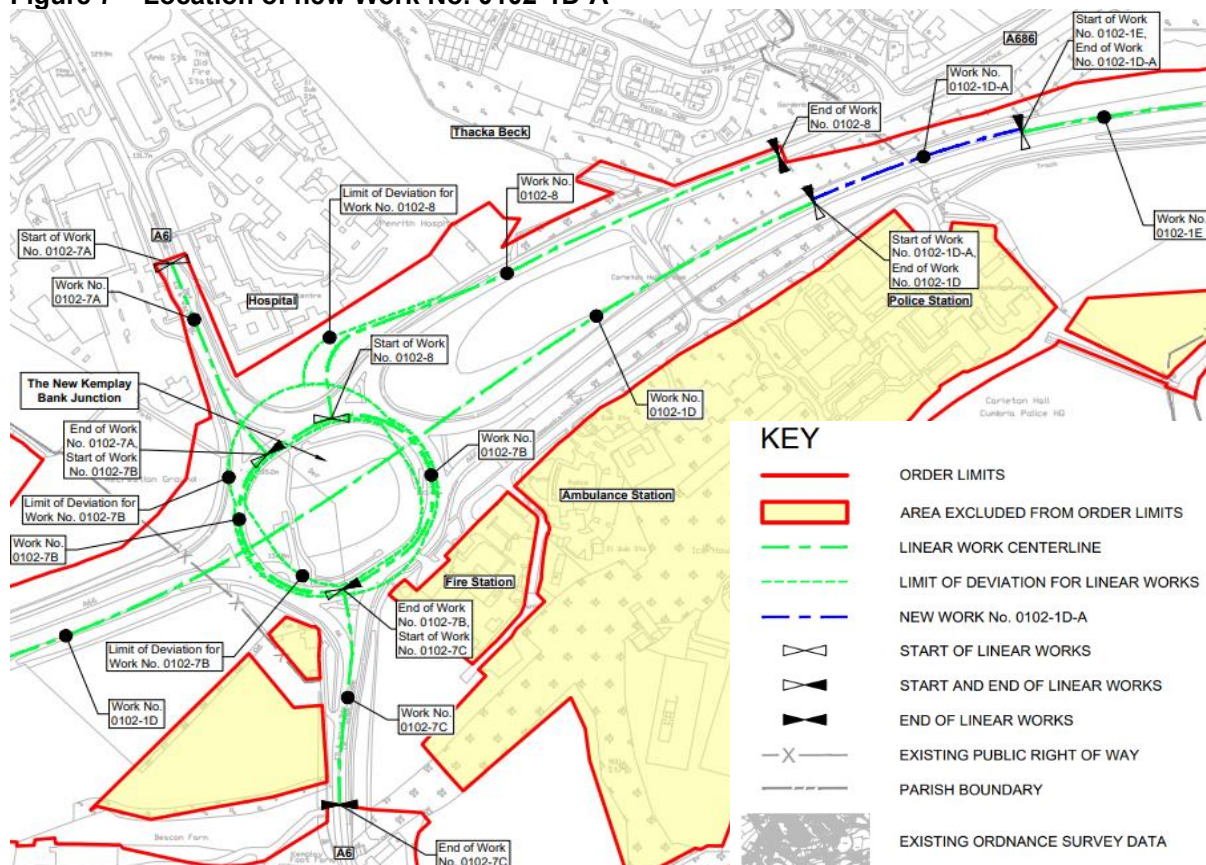
2.7.3. The following plans certified under article 49 of the Order and listed in Schedule 10 thereto, show the alignment of the A66 beneath Kemplay Bank:

- Engineering section drawings: plans and profiles, comprising the following:
 - Engineering Section Drawings (Plan and Profiles) Scheme 0102 M6 Junction 40 to Kemplay Bank Sheet 2 of 6 HE565627-AMY-HAC-S0101-DR-CH-101001
- Works plans, comprising the following:
 - Works Plans Scheme 0102 M6 Junction 40 to Kemplay Bank Sheet 2 of 2 HE565627-AMY-HAC-S0102-DR-CH-101201

Description of Proposed Change

- 2.7.4. The Proposed Change is an amendment to a section of the centreline of Work No. 0102-1E. At the start of Work No 0102-1E to Chainage 11+400, the centreline would be re-located 2.5 metres to the north of its current location. The new section would be given a new Work Number and Work Number description under 0102-1D-A. The standard LoD for linear works (as set out in article 7 of the Order) would apply to the new Work No. 0102-1D-A.

Figure 7 – Location of new Work No. 0102-1D-A



Rationale for the Proposed Change

- 2.7.5. A change to the centreline in Work no. 0102-1E is required to ensure the design is compliant with highway standards associated with the realignment of Kemplay Bank roundabout and the tie-in to the existing A66. As a consequence, the alignment of the road needs to move further north than is currently possible under the DCO as made.
- 2.7.6. The Proposed Change was led by the Applicant but was developed following engagement with WaFC. Consultation evidence is not required as National Highways (the Applicant) is the landowner.

Changes to Certified Documents

- 2.7.7. Annex B lists the certified documents that must be updated as a result of this Proposed Change, together with the revised drawing sheets accompanying this application, which would replace the corresponding certified versions.

Amendments to DCO

- 2.7.8. There are no amendments to DCO Schedule 2 as a result of Proposed Change 6. The Proposed Change requires the addition of a new Work Number (Work No. 0102-1D-A) and new description to accompany it into Schedule 1 of the DCO, as shown in the mark-up of the DCO and Amendment Order accompanying this Change Application.

2.8. Proposed Change 7 – B6262 Combined Public Rights of Way and Private Means of Access

The Authorised Development provides for new public rights of way and private means of access between the junction of the improved A66 with the B6262 and the Lightwater Watercourse.

The Authorised Development

- 2.8.1. The first combined private means of access (as shown on Figure 8 below), and cycle track and public right of way subject to this Proposed Change relates to the section between the B6262, running parallel along the south of the A66 before crossing over the A66 and continuing parallel with the north side of the A66. This is authorised under Work No. 03-7A in Part 2 of Schedule 1 of the Order.
- 2.8.2. The second combined private means of access, cycle track and public right of way subject to this Proposed Change relates to the section running between the site of the former Llama Karma Kafe and the first combined private means of access, cycle track and public right of way to the south of the A66 crossing with a spur going to the Countess Pillar. This is authorised under Work No. 03-09 in Part 2 of Schedule 1 of the Order.
- 2.8.3. The following plans certified under article 49 of the Order and listed in Schedule 10 thereto, define the alignment of the private means of access, cycle track and footpath:
- Engineering section drawings: cross sections, comprising the following:
 - Engineering Section Drawings (Cross Sections) Scheme 03 Penrith to Temple Sowerby Sheet 1 of 3 HE615323-JAC-HAC-03A_AL_SCHEM-DR-CH-201101
 - Engineering section drawings: plans and profiles, comprising the following:
 - Engineering Section Drawings (Plans and Profiles) Scheme 03 Penrith to Temple Sowerby Sheet 1 of 5 HE615323-JAC-HAC-03A_AL_SCHEM-DR-CH-201001

- Rights of way and access plans, comprising the following:
 - Rights of Way and Access Plans Scheme 03 Penrith to Temple Sowerby Sheet 1 of 4 HE615323-JAC-HAC-03A_AL_SCHEM-DR-CH-201401
- Works plans, comprising the following:
 - Works Plans Scheme 03 Penrith to Temple Sowerby Sheet 1 of 4 HE615323-JAC-HAC-03A_AL_SCHEM-DR-CH-201201
- Traffic regulation measures: clearways and prohibitions, comprising the following:
 - Traffic Regulation Measures (Clearways and Prohibitions) Plans Scheme 03 Penrith to Temple Sowerby Sheet 1 of 4 HE615323-JAC-HAC-03A_AL_SCHEM-DR-CH-201601
- Traffic regulation measure: speed limits, comprising the following:
 - Traffic Regulation Measures (Speed Limits) Plans Scheme 03 Penrith to Temple Sowerby Sheet 1 of 4 HE615323-JAC-HAC-03A_AL_SCHEM-DR-CH-201501

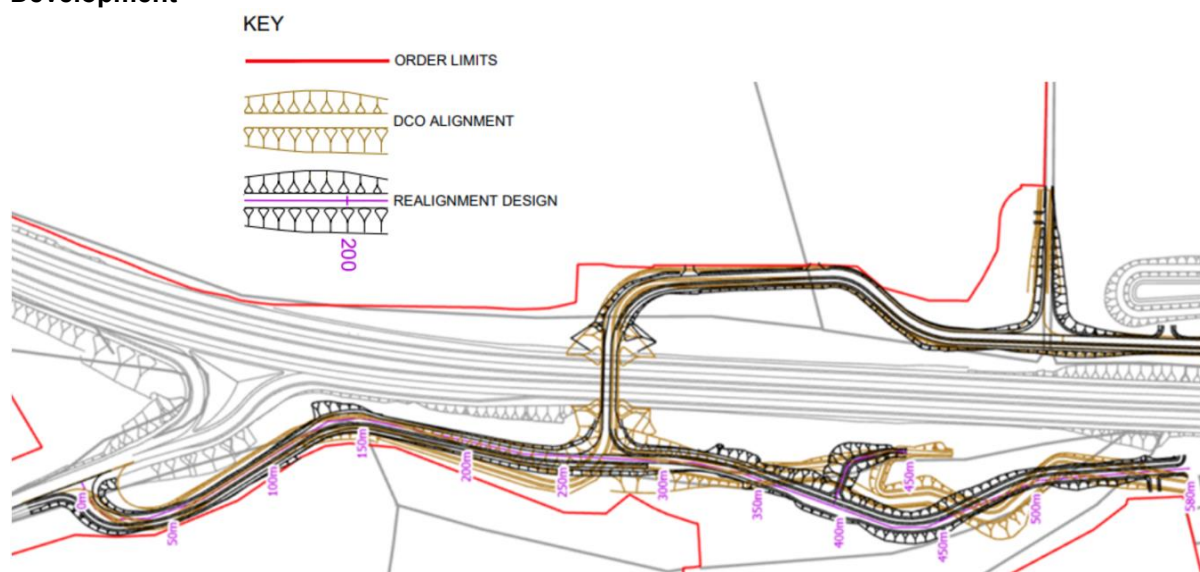
Description of Proposed Change

- 2.8.4. The Proposed Change involves the realignment of various lengths of combined PRow and PMA:
- on the south side of the A66, running between the B6262 and the site of the former Llama Karma Kafe, and providing access to the Countess Pillar which is located between them (comprising part of Work No. 03-7A, and both Work Nos. 03-9A and 03-9B); and
 - crossing over the A66, then running along the north side of the A66 in an easterly direction (comprising part of Work No. 03-7A and part of Work No. 03-7B).
- 2.8.5. Additionally, the Proposed Change on the south side of the A66 comprises the re-designation of a section of footpath to cycle track to form a continuous cycle track along the entirety of the route between the site of the former Llama Karma Kafe, the Countess Pillar and the B6262, amending the previously consented proposals which comprised mainly cycle track, but which also included a short length of footpath on the approach to the Countess Pillar.
- 2.8.6. Part of the realignment of Work No. 03-7A involves its commencement point, which is proposed to be relocated 8 metres to the south of the junction of the B6262 with the A66, in comparison with the commencement point in the previously consented proposal. This would be relocated beyond the current lateral LoD for Work No.03-07A, which are 3 metres to the north and to the south of the centre of the PMA.
- 2.8.7. Beyond that, Proposed Change 7 includes further changes to the alignment of Work No. 07-3A, which at some points exceed the lateral LoD

for Work No. 03-7A, which is 3 metres either side of the centreline. Proposed Change 7 therefore includes a partial realignment of the centreline for Work No. 03-7A, to which the standard 3 metre lateral LoD (as set out in article 7 of the Order) would subsequently apply.

- 2.8.8. Amendments of the same nature are also proposed to the alignment of the centreline of Work Nos. 03-7B, 03-9A and 03-9B. The proposed realignments of these combined PMAs and cycle tracks extend beyond the LoD for each of the relevant numbered works. The LoD for these linear works (as set out in article 7 of the Order) would continue to apply to their realigned centrelines.

Figure 8 – Comparison showing the Proposed Change overlaid on the Authorised Development



Rationale for the Proposed Change

- 2.8.9. The proposed changes to the combined PMA and cycle tracks would reduce the need to acquire third party land, reduce work in the vicinity of a Scheduled Monument and reduce work around watercourses and avoid the need to construct a retaining wall.
- 2.8.10. Additionally, the change to Work No. 03-07A would result in a safer junction arrangement between the B6262 and the combined PMA and cycle track by moving the junction further away from the B6262/A66 junction, whilst the realigned combined PMA and cycle track would encourage reduced vehicle speeds across the Brougham Overbridge.
- 2.8.11. The proposed changes to Work Nos. 03-09A and 03-09B would provide a more direct route between the site of the former Llama Karma Kafe, the Countess Pillar and the B6262 whilst providing a continuous cycle track, removing the need for cyclists to dismount.
- 2.8.12. Please refer to Annex C7 and C8 for evidence of correspondence with landowners in relation to Proposed Change 7.

Changes to Certified Documents

- 2.8.13. Annex B lists the certified documents that must be updated as a result of this Proposed Change, together with the revised drawing sheets accompanying this application, which would replace the corresponding certified versions.

Amendments to DCO Schedule 1 and Schedule 2

- 2.8.14. Work No. 03-9B in Schedule 1 paragraphs (i) and (ii) have minor textual amendments as a result of this Proposed Change.
- 2.8.15. Schedule 2 to the DCO is proposed to be amended in order to reflect the Proposed Change. Table 6, below, provides a summary of these proposed amendments. A draft Amendment Order is submitted as part of this application.
- 2.8.16. Schedule 2 details the permanent stopping up of highways and private means of access and the provision of new highways and private means of access. The provisions described in the Schedule correspond to and are shown on the Rights of Way and Access Plans.
- 2.8.17. Part 1 of Schedule 2 is highways to be stopped up for which a substitute is to be provided, and new highways which are otherwise to be provided. Part 3 of Schedule 2 is private means of access to be stopped up, for which a substitute is to be provided, and new private means of access which are otherwise to be provided.

Table 6 – Proposed Change 7 Amendments to DCO Schedule 2

Column (2) 'Highway to be stopped up'	Summary of changes to '(3) Extent of stopping up'	Summary of changes to '(4) New highway to be substituted/provided'
Scheme 03 – Penrith to Temple Sowerby – Rights of Way and Access Plans Sheets 1 and 2 – Schedule 2 Part 1		
Footpath	No Change.	Reference D: The designation of the new highway has changed from 'footpath' to 'cycle track'.
–	–	Reference E: The length of new cycle track being provided has changed.
–	–	Reference L: The designation of the new highway has changed from 'footpath' to 'cycle track'.
–	–	Reference M: This has been deleted.

Column (2) <i>'Private means of access to be stopped up'</i>	Summary of changes to <i>'(3) Extent of stopping up'</i>	Summary of changes to <i>'(4) New private means of access to be substituted/provided'</i>
Scheme 03 – Penrith to Temple Sowerby – Rights of Way and Access Plans Sheets 1 and 2 – Schedule 2 Part 3		
–	–	Reference 5: The description of new PMA has been amended to show it is integrated into PRow References B, D and E (New Cycle Track).
Reference g	No change.	Reference 8: The location of the new PMA has changed.

2.9. Proposed Change 8 – Powis House Private Means of Access

- 2.9.1. The Order authorises the provision of new footpaths and private means of access (the Trout Beck Bridge south shore maintenance track) between the existing A66 and the proposed A66. This includes the construction of a new private means of access and footpath passing beneath the Trout Beck viaduct and connecting to the existing Footpath 341/017.

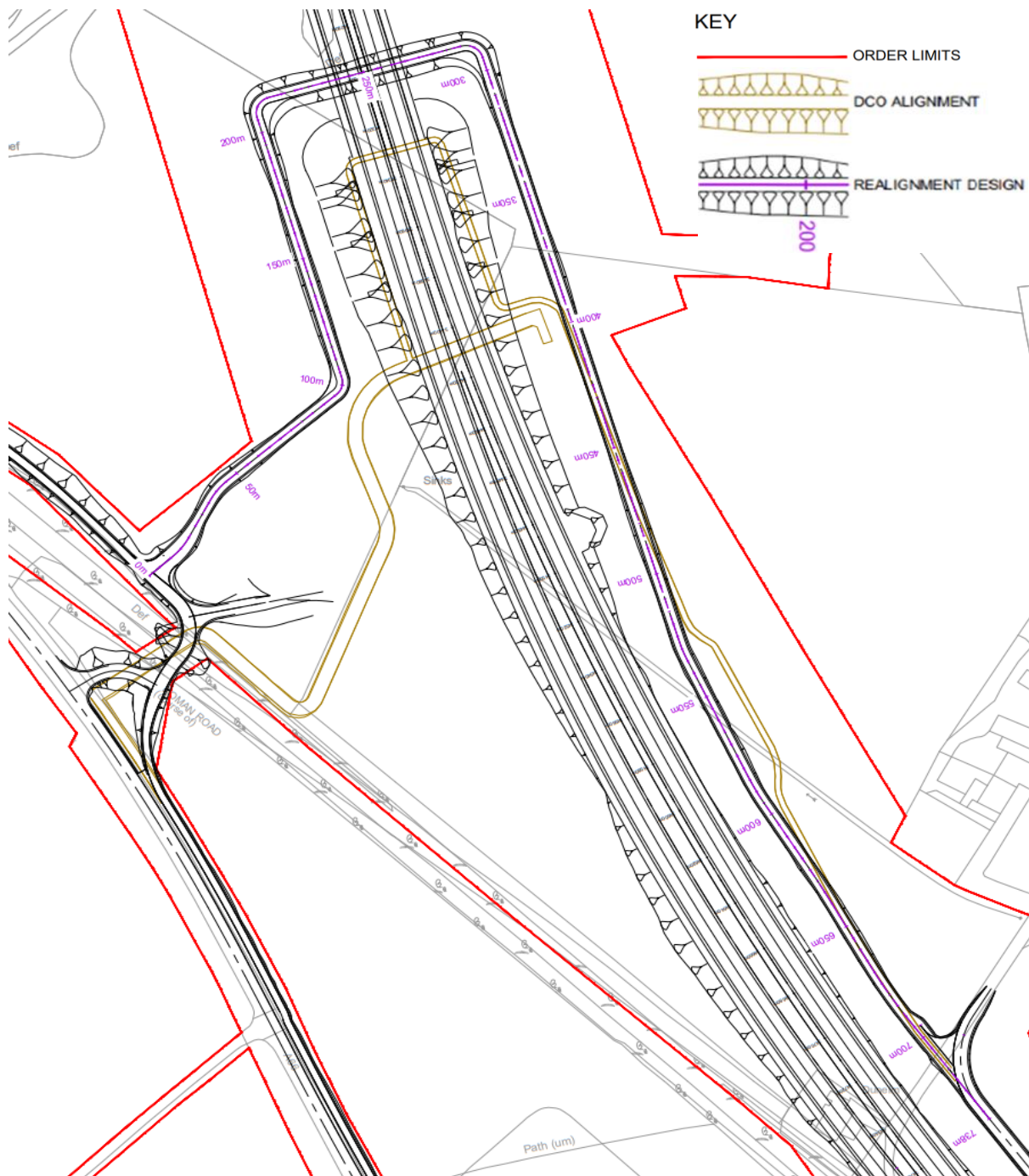
The Authorised Development

- 2.9.2. The realignment of Powis House which is subject to this Proposed Change is authorised under Work No. 0405-14 in Part 3 of Schedule 1 of the Order.
- 2.9.3. The following plans certified under article 49 of the Order and listed in Schedule 10 thereto, define the Powis House PMA Change:
- Engineering section drawings: plans and profiles, comprising the following:
 - Engineering Section Drawings (Plans and Profiles) Scheme 0405 Temple Sowerby to Appleby Sheet 4 of 24 HE565627-AMY-HAC-S0405-DR-CH-301004
 - Engineering Section Drawings (Plans and Profiles) Scheme 0405 Temple Sowerby to Appleby Sheet 11 of 24 HE565627-AMY-HAC-S0405-DR-CH-301011
 - Rights of way and access plans, comprising the following:
 - Rights of Way and Access Plans Scheme 0405 Temple Sowerby to Appleby Sheet 4 of 7 HE565627-AMY-HAC-S0405-DR-CH-301404

- Rights of Way and Access Plans Scheme 0405 Temple Sowerby to Appleby Sheet 5 of 7 HE565627-AMY-HAC-S0405-DR-CH-301405
- Traffic regulation measures (clearways and prohibitions) plans, comprising the following:
 - Traffic Regulation Measures (Clearways and Prohibitions) Plans Scheme 0405: Temple Sowerby to Appleby Sheet 4 of 7 HE565627-AMY-HAC-S0405-DR-CH-301604
 - Traffic Regulation Measures (Clearways and Prohibitions) Plans Scheme 0405: Temple Sowerby to Appleby Sheet 5 of 7 HE565627-AMY-HAC-S0405-DR-CH-301605
- Traffic regulation measures (speed limits) plans, comprising the following:
 - Traffic Regulation Measures (Speed Limits) Plans Scheme 0405: Temple Sowerby to Appleby Sheet 4 of 7 HE565627-AMY-HAC-S0405-DR-CH-301504
 - Traffic Regulation Measures (Speed Limits) Plans Scheme 0405: Temple Sowerby to Appleby Sheet 5 of 7 HE565627-AMY-HAC-S0405-DR-CH-301505
- Works plans, comprising the following:
 - Works Plans Scheme 0405 Temple Sowerby to Appleby Sheet 4 of 7 HE565627-AMY-HAC-S0405-DR-CH-301204

Description of Proposed Change

- 2.9.4. The Proposed Change comprises realigning the new footpath and private means of access approximately 20 metres to the northwest of its current authorised location. Such realignment is required in consequence of the shortening of the Trout Beck viaduct, which is pursuant to a detailed design change. The proposed re-alignment of the footpath and private means of access would exceed the current lateral LoD for Work No.0405-14, which is 3 metres to the north and south of the centreline of this numbered work.

Figure 9 - Comparison showing the Proposed Change overlaid on the Authorised Development

Rationale for the Proposed Change

- 2.9.5. The need for the realignment of the new footpath and PMA, approximately 20 metres to the northwest of its authorised location, is a consequence of the shortening of the Trout Beck viaduct through detailed design. Neither the footpath nor the PMA, as currently authorised, can accommodate the detailed design change; hence the proposed non-material change.

- 2.9.6. The two impacted landowners were consulted during the development of the design and the meeting minutes showing no objection to the Proposed Change is evidenced in Annex C5 and C6.

Changes to Certified Documents

- 2.9.7. Annex B lists the certified documents that must be updated as a result of this Proposed Change, together with the proposed amended drawings, which would replace the corresponding certified version.

Amendments to DCO Schedule 2

- 2.9.8. It is proposed that Schedule 2 to the DCO is amended in order to reflect the Proposed Change. Table 7, below, provides a summary of these proposed amendments. A draft Amendment Order has been submitted with this application. There are no changes required to Schedule 1.
- 2.9.9. Schedule 2 details the permanent stopping up of highways and private means of access and the provision of new highways and private means of access. The provisions described in the Schedule correspond to and are shown on the Rights of Way and Access Plans.
- 2.9.10. Part 1 of Schedule 2 is the highways to be stopped up for which a substitute is to be provided, and new highways which are otherwise to be provided. Part 3 of Schedule 2 is the PMAs to be stopped up, for which a substitute is to be provided, and new PMAs which are otherwise to be provided.

Table 7 – Proposed Change 8 Amendments to DCO Schedule 2

Column (2) <i>'Highway to be stopped up'</i>	Summary of changes to '(3) Extent of stopping up'	Summary of changes to '(4) New highway to be substituted/provided'
Scheme 0405 – Temple Sowerby to Appleby – Rights of Way and Access Plans Sheets 4 and 5 – Schedule 2 Part 1		
Footpath 317/009 and Footpath 341/017	No change.	Reference E: Description has been amended to include Point M and length of the new footpath has changed. Reference C*: No change.

Column (2) <i>'Private means of access to be stopped up'</i>	Summary of changes to '(3) <i>Extent of stopping up</i> '	Summary of changes to '(4) <i>New private means of access to be substituted/provided</i> '
Scheme 0405 – Temple Sowerby to Appleby – Rights of Way and Access Plans Sheets 4 and 5 – Schedule 2 Part 3		
–	–	Reference 49: The length of the new private means of access has changed.

3. Assessment of the Proposed Non-Material Changes

3.1.1. The Department for Communities and Local Government (DCLG) 2015 Guidance on Changes to Development Consent Orders ('DCLG Guidance') acknowledges that there is no specific legal definition of "non-material" but highlights certain characteristics that indicate whether a change to a consent is more likely to be treated as material. The characteristics discussed in paragraphs 9 to 16 of the DCLG Guidance cover the following matters:

- (i) Environmental Statement
- (ii) Habitats and Protected Species
- (iii) Compulsory Acquisition
- (iv) Impact on Business and Residents

3.1.2. The potential impacts and materiality of the Proposed Changes in relation to the above matters have been considered below.

Table 8 – Summary of the Assessment of Materiality

Item	Assessment	Material? Yes or No
(i) Environmental Statement	<p>The DCLG Guidance states that a change should be treated as material if it would require an updated Environmental Statement (ES) (from that at the time the original development consent order was made) to take account of new, or materially different, likely significant effects on the environment.</p> <p>The potential impacts of the Proposed Changes have been assessed with regard to the relevant ES chapters for the made DCO. The changes will not result in any new, or materially different, likely significant effects on the environment either individually or cumulatively. This assessment is detailed in the Environmental Report accompanying the application, and summarised in sections 3.1.3 – 3.1.8 of this Application Statement.</p>	No
(ii) Habitats and Protected Species	<p>The DCLG Guidance states that a change to a DCO is likely to be material if it would invoke the need for a Habitats Regulations Assessment (HRA) or the need for a new or additional licence in respect of European Protected Species.</p> <p>The Proposed Changes do not invoke the need for an updated or new HRA or require the need for a new or additional licence in respect of European Protected Species. This is because the Proposed Changes will have no new impacts on habitats and protected species or result in any new or different effects on habitats or protected species.</p>	No
(iii) Compulsory Acquisition	<p>The DCLG Guidance states that a change should be treated as material if it would authorise the compulsory acquisition of any</p>	No

Item	Assessment	Material? Yes or No
	land, or an interest in, or rights over land, that was not authorised through the existing DCO. The Proposed Changes will not require any additional compulsory acquisition of land, or an interest in land, or rights over land beyond what is already authorised in the Order.	
(iv) Impact of Business and Residents	<p>The DCLG Guidance states that the potential impacts of the Proposed Changes on local people and businesses will be a consideration in determining whether a change is material. It states that there may be situations where the impact of the change on local people and businesses will be sufficient to indicate that the change should be considered as being material. Examples given in the DCLG Guidance include impacts relating to visual amenity from changes to the size or height of buildings, impacts on the natural or historic environment and impacts arising from additional traffic.</p> <p>The impacts of the Proposed Changes on businesses and local residents are not considered sufficient to indicate that the Proposed Changes should be considered to be material, given the minor nature of the Proposed Changes, the agreement from land owners and the lack of objections.</p> <p>The impact of the Proposed Changes on local residents and businesses is detailed in paragraphs 3.1.11 to 3.1.14 of this Application Statement.</p>	No

Environmental Review and Assessment

- 3.1.3. To be considered non-material, the Proposed Changes must not give rise to any new, or materially different, likely significant effects on the environment compared to those reported in the Environmental Statement (ES) supporting the Order.
- 3.1.4. National Highways has also considered whether the Proposed Changes would constitute 'EIA Development' for the purposes of the Infrastructure Planning (Environmental Impact Assessment) Regulations 2017 (the '2017 Regulations'). The Proposed Changes do not constitute either Schedule 1 development or Schedule 2 development. Paragraph 13 of Schedule 2 of the 2017 Regulations provides that a change or extension to a Schedule 1 development which has already been authorised will be Schedule 2 development only if *"the change or extension may have significant adverse effects on the environment"*. In considering whether or not that is likely, the changes are not to be assessed in isolation. They should be considered by looking at the overall effect of the Proposed Changes on the Project, and identifying whether the whole, as modified, has or is likely to have other significant effects which need to be taken into account (i.e. significant effects which were not identified in the original assessment).

- 3.1.5. The ES for the made Order has been reviewed in light of the Proposed Changes as part of the Environmental Report accompanying this Change Application. This review of the ES is supported by the information gathered since the DCO was made. The review and supporting assessment work has found that the Proposed Changes are likely to have no more than negligible impacts and would not change the assessment conclusions presented in the ES for the made Order. Table 9 below summarises the conclusions reached. For certain topics, specific Proposed Changes are highlighted where considered necessary to the ES topic along with the general combined effect of all the Proposed Changes. The full assessment is contained in the Environmental Report accompanying the Non-Material Change Application.

Table 9 – Summary of Environmental Report’s conclusions for each ES topic

Environmental Statement Chapter	Reviewed?	Potential Effects of the Proposed Changes	Further Assessment required?
Chapter 5: Air Quality APP-048	Yes	The Proposed Changes have potential for slight reductions in traffic speed. Proposed Change 2 will move traffic using Green Lane away from sensitive receptors as per the ES. There would be no effect on the Air Quality conclusions in the original ES.	No
Chapter 6: Biodiversity APP-049	Yes	Proposed Change 3 will result in the retention of additional mature vegetation. The Proposed Changes involve minor and insignificant changes in the physical layout of side roads which are considered to have no effect on the Biodiversity conclusions in the original ES.	No
Chapter 7: Climate APP-050	Yes	Proposed Changes 2, 3, 4 and 5 will require fewer earthworks, therefore less materials used for construction and will result in a reduction of embodied carbon. The Project has been designed to account for future projected climate change. The Proposed Changes are minor in scale and are not considered to have any effect on the Climate conclusions in the original ES.	No
Chapter 8: Cultural Heritage APP-051	Yes	The Proposed Changes involve minor and insignificant changes in the physical layout of side roads which are considered to have no effect on the Cultural Heritage conclusions in the original ES.	No
Chapter 9: Geology and Soils APP-052	Yes	Proposed Changes 2, 3, 4 and 5 will require fewer earthworks and will result in reduced potential contamination and soils impacts. The Proposed Changes are minor in scale and are considered to have no effect on the Geology and Soils conclusions in the original ES.	No

Environmental Statement Chapter	Reviewed?	Potential Effects of the Proposed Changes	Further Assessment required?
Chapter 10: Landscape and Visual APP-053	Yes	Proposed Changes 1 and 5 will result in minor reductions in the visibility of structures which will reduce visual impacts. The Proposed Changes are minor in scale and are not considered to have any effect on the Landscape and Visual conclusions in the original ES.	No
Chapter 11: Material Assets and Waste APP-054	Yes	Proposed Changes 2, 3, 4 and 5 will require fewer earthworks and will result in a reduction in materials and waste. The scale of the Proposed Changes are minor and will not lead to any significant changes in the Project's use of materials. The Proposed Changes are not considered to have any effect on the Material Assets and Waste conclusions in the original ES.	No
Chapter 12: Noise and Vibration APP-055	Yes	Proposed Change 2 will move traffic using Green Lane away from noise sensitive receptors. The Proposed Changes have potential for slight reductions in traffic speed and but will not alter traffic levels. The Environmental Report concludes that the Proposed Changes will have no effect on the Noise and Vibration conclusions reached in the original ES.	No
Chapter 13: Population and Human Health APP-056	Yes	Proposed Change 4 will reduce agricultural land severance. The Proposed Changes will not lead to any significant changes to Population and Human Health. There is no effect on the Population and Human Health conclusions in the original ES.	No
Chapter 14: Road Drainage and the Water Environment APP-057	Yes	Proposed Change 1 Cross Street involves the lowering of the carriageway elevation, which is likely to marginally increase the groundwater dewatering required from the superficial deposits and reduce the groundwater level in this area during the operational phase; however, these changes will have no effect on the Road Drainage and the Water Environment conclusions in the original ES.	No
Chapter 15: Cumulative Effects APP-058	Yes	Refer to paragraphs 3.1.7 and 3.1.8.	Yes, refer to Section 4 of the accompanying Environmental Report.

- 3.1.6. The review of ES for the made Order, as described in the accompanying Environmental Report, Section 6 has identified that there would be no change in the magnitude of impacts and no change in significance of likely effects concluded in all ES Chapters as a result of the Proposed Changes. As such, the Proposed Changes would result in no new, or materially different, likely significant environmental effects.
- 3.1.7. A cumulative assessment of the Proposed Changes has been carried out and is presented in full in Section 4 of the accompanying Environmental Report. The cumulative assessment follows the standard assessment methodology for cumulative effects for highways schemes as set out in Design Manual for Roads and Bridges (DMRB) LA 104 Environmental assessment and monitoring (Highways England, 2020). This is the same approach taken for the cumulative assessment within the ES for the made Order.
- 3.1.8. There are no significant cumulative effects anticipated as a result of the Proposed Changes which would result in any new, or materially different, significant effects to those identified in the ES for the made Order. As a result, no mitigation measures other than those already set out in the original ES's Chapters 5 to 14 and the relevant Environmental Management Plans are required for the Project.

Habitats and protected species

- 3.1.9. A number of European Protected Species licences have been granted with respect to construction of the Project. Based on currently available information, it is not anticipated that the Proposed Changes would require any changes to these licences, or any new licences. Consequently, no updated or new Habitats Regulations Assessment is required for the Proposed Changes.

Land acquisition

- 3.1.10. No further compulsory acquisition powers are required in relation to the Proposed Changes. Changes in the Project's layout as a result of the Proposed Changes would reduce the amount of land required to be acquired in order to construct the Project.

Impact on businesses and residents

- 3.1.11. There would be a potential beneficial impact on a farming business as Proposed Change 4 would reduce severance of an agricultural field. There would be a potentially beneficial impact to residents associated with Proposed Change 1 and the realignment of Cross Street as this realignment would encourage a reduction in vehicle speeds near Kirkby Thore Primary School.

- 3.1.12. Proposed Changes 1, 2, 3, 4, 5, 7 and 8 involve alterations to private means of access, which have the potential to impact local businesses and residents that rely on these Private Means of Access. These Proposed Changes are sought to improve the design and layout of the Private Means of Access, reduce the amount of third party land required to construct the Project and simplify construction.
- 3.1.13. The potential impacts on affected businesses and residents would be positive or neutral, as highlighted by the lack of objection comments from landowners (evidenced in Annex C) and through the Community Liaison Group Meetings (detailed in Section 4).
- 3.1.14. The Proposed Changes are of a minor and localised nature. The accompanying Environmental Report, as summarised in the above paragraphs, indicates that the Proposed Changes would have no materially new, or materially different, environmental effects compared to those identified in the ES for the made Order. The Proposed Changes are considered to result in no material impact to local businesses and residents.

4. Pre-application Engagement

- 4.1.1. National Highways undertook an extensive pre-application consultation on the original DCO application. The pre-application engagement and consultation is set out in the Consultation Report (application ref: APP-252¹) submitted with the DCO application. Following the original application being accepted for examination, National Highways publicised the accepted application and notified the prescribed relevant bodies and persons in accordance with the requirements of section 56 of the Planning Act 2008. Overall, 235 interested parties submitted relevant representations and registered to participate in the examination of the original DCO application. Careful consideration has been given to who may be interested in the Non-Material Change Application.
- 4.1.2. To identify any potential concerns about the non-material change proposals and any parties who may be interested in the non-material change proposals, National Highways has reviewed submissions on the original application to identify references to matters linked to the proposals in this Non-Material Change Application. Pre-application engagement with the following parties was undertaken before submitting this Non-Material Change Application:
- Westmorland and Furness Council (WaFC), in its role as the Local Highway Authority and Local Planning Authority;
 - Landowners (freeholders) whose interests are directly affected by the Proposed Changes;
 - Community Liaison Groups; and
 - Parish Councils.
- 4.1.3. Online sessions were held with the Penrith to Kemplay, Kemplay Bank to Temple Sowerby, Kirkby Thore to Long Marton, and Appleby to Brough Community Liaison Groups in November 2024, in partnership with WaFC. In addition, the invitation was extended beyond the route to all 55 Parish Councils that are either on the A66 route or may be affected by the Project. Each Community Liaison Group includes local councillors from affected wards, parish councillors, community representatives, businesses and residents.
- 4.1.4. An additional online session with each of the Community Liaison Groups listed in paragraph 4.1.3 was held relating to Proposed Change 8 (Powis House Private Means of Access) in August 2025, in partnership with WaFC. The invitation was also extended to all 55 Parish Councils.

¹ <https://infrastructure.planninginspectorate.gov.uk/wp-content/ipc/uploads/projects/TR010062/TR010062-000215-4.4%20Consultation%20Report.pdf>

- 4.1.5. These events enabled attendees to see the proposed non-material changes and ask any questions of the designers. There were no specific questions raised by those attending regarding the proposed non-material changes.

Westmorland and Furness Council

- 4.1.6. WaFC, as the Local Highway Authority and Local Planning Authority, has been engaged during the development of the detailed design and provided review comments on draft design drawings for all of the Proposed Changes:

- Proposed Change 1 Cross Street
- Proposed Change 2 Green Lane
- Proposed Change 3 Long Marton
- Proposed Change 4 Access track at Kirkby Thore
- Proposed Change 5 B6542 and Spitals Farm Access
- Proposed Change 6 Kemplay Bank
- Proposed Change 7 B6262 Combined Public Right of Way and Private Means of Access
- Proposed Change 8 Powis House Private Means of Access.

- 4.1.7. Proposed Change 1 (Cross Street) and Proposed Change 3 (Long Marton) were developed following a request by WaFC (in its role as the LHA and LPA) to design these roads to encourage reduced speeds.

- 4.1.8. Correspondence with the WaFC, confirming their agreement to all of the Proposed Changes, is provided as Annex C to this Application Statement.

Landowners

- 4.1.9. All landowners (freeholders) whose land is directly affected by the Proposed Changes, in that the Proposed Changes take place on their land, have been consulted during the design development of all eight Proposed Changes. The need for Proposed Change 2 (Green Lane) has arisen from engagement with the landowner and, if approved, would reduce farm severance for that landowner. Proposed Change 7 (B6262 Combined Private Means of Access) has arisen as a result of engagement with the landowner and United Utilities around their vehicle access requirements. The landowners have not objected to the Proposed Changes that are relevant to them (see Annex C).

Community Liaison Groups (CLGs) & Parish Council

- 4.1.10. To give local communities an opportunity to hear about the Proposed Changes ahead of the submission of this Change Application, online design sessions were arranged in partnership with Westmorland and Furness Council Community Liaison Team, as summarised in Table 10 below.

- 4.1.11. Each Community Liaison Group included local councillors from affected wards, parish councillors, community representatives, businesses, and residents.
- 4.1.12. The sessions gave the Community Liaison Groups an opportunity to see the designs in more detail and to ask questions of the designers.
- 4.1.13. During these sessions, the designers introduced the audience to the non-material changes and explained the process for requesting approval for Proposed Changes. They encouraged questions but the audience made no comments regarding these changes.
- 4.1.14. No comments have been received following these events.

Table 10 – Community Liaison Group Meetings

Community Liaison Group/ Parish Council area	Date	Non-material change(s) discussed
Kirkby Thore to Long Marton	11/12/2024	Proposed Change 1 Cross Street: amendments to vertical and horizontal arrangement of Cross Street. Proposed Change 2 Green Lane: amendment to the alignment of Green Lane. Proposed Change 3 Long Marton: amendment to the alignment of Morland Road to reduce bridge span and amendment to the alignment and cross section of Long Marton bridge. Proposed Change 4 Access track at Kirkby Thore: removal of a private means of access track through farmland. Proposed Change 5 B6542 and Spitals Farm access re-alignment: amendment to the alignment of a private means of access and reduction in length of underpass.
Kemplay Bank to Temple Sowerby	12/12/2024	Proposed Change 6 Kemplay Bank: amendment to DCO centreline of part of Work number 0102-1E. Proposed Change 7 B6262 Combined Private Means of Access: amendments to the alignment of the B6262 junction with the A66 and alignment of three private means of access.
Kirkby Thore to Long Marton	26/08/2025	Proposed Change 8: Powis House private means of access: amendment to the alignment of a private means of access.

- 4.1.15. National Highways wrote to the Secretary of State seeking consent not to consult certain parties about this Change Application, in accordance with regulation 7(3) to the Infrastructure Planning (Changes to, and Revocation of, Development Consent Orders) Regulations 2011, as amended. The Secretary of State has subsequently confirmed who must be consulted on the Change Application. Copies of National Highways' letter to the Secretary of State and the reply thereto are in Annex A of this Application Statement.

5. Conclusion

- 5.1.1. National Highways has considered the DCLG Guidance and assessed the proposed non-material changes in accordance with the characteristics set out therein:
- The environmental effects associated with the Proposed Changes have been assessed and it has been concluded that there would be no new, or materially different, likely significant effects on the environment as a result of the Proposed Changes.
 - The Proposed Changes do not invoke the need for a new Habitats Regulations Assessment or new or additional protected species licences.
 - The Proposed Changes do not require authorisation for the compulsory acquisition of additional land or additional interests in land or additional rights over land beyond that which has already been authorised by the Order.
 - The impacts of the Proposed Changes on local residents and businesses are minor in their scope and nature in that they are non-material, and are not considered to be sufficient enough to indicate a material change.

Annex A. Parties to be consulted on the Non-Material Change Application

Letter to the Secretary of State seeking confirmation on who must be consulted on this Non-Material Change Application



Your ref: TR010062

Transport Infrastructure Planning Unit
Department for Transport
Zone 1/14 – 18
Great Minster House
33 Horseferry Road
London
SW1P 4DR

Project Manager
A66 Northern Trans-Pennine Project
National Highways
9th Floor
Piccadilly Gate
Store Street
Manchester
M1 2WD
Tel: 0300 123 5000

28 August 2025

Dear Sir and Madam,

Application for proposed non-material change to the A66 Northern Trans-Pennine Development Consent Order 2024 (SI 2024 No. 360)

The Infrastructure Planning (Changes to, and Revocation of, Development Consent Orders) Regulations 2011: Regulation 7(3) – request for exemption from consulting certain persons on the above application

On 21 June 2022, National Highways submitted an application for a development consent order (DCO) for the A66 Northern Trans-Pennine project. The A66 Northern Trans-Pennine Development Consent Order 2024 (SI 2024 No.360) (the Order) was subsequently made by the Secretary of State for Transport on 7 March 2024. Documents relating to the decision stage can be found on the Planning Inspectorate's national infrastructure website.

National Highways intends to make an application for a non-material amendment to the A66 Northern Trans-Pennine DCO. The application will seek authorisation to make the following changes to the project (NB: Changes 1 to 5 and 8 are proposed on Scheme 0405; Change 6 on Scheme 0102; and Change 7 on Scheme 03):

Change 1 – Cross Street

Cross Street is a local road (C3030) in Kirkby Thore, passing Kirkby Thore Primary School, then heading northwards towards Newbiggin. As the alignment of the new A66 will cut across Cross Street, the Order authorises the realignment of Cross Street and the provision of a new overbridge crossing the new A66.

The proposed change comprises the realignment of the overbridge, approaches, and the connecting junctions and private means of access. The lateral alignment of Cross Street will be adjusted beyond the lateral limits of deviation (LoD) for Work No. 0405-7,

which are currently 3m to the east, and to the west to the extent of the fine dashed green line shown on the works plans.

The Priest Lane junction with Cross Street will be relocated beyond the lateral LoD for Work No.0405-6C, which is to the east to the extent of the fine dashed green line shown on the works plans and to the west by 3m.

The new Bridleway between the realigned Cross Street and Bridleway 336/018 will be relocated beyond the LoD for Work No. 0405-8, which are 3m to the east and the west to the extent of the fine dashed green line shown on the works plans.

Proposed Change 1 (Cross Street) therefore comprises the realignment of the centreline for each of Work Nos. 0405-7, 0405-6C and 0405-8. The current LoD (as stated above) would then apply to the realigned centrelines for those numbered works.

Change 2 – Green Lane

Green Lane comprises part of Footpath 336/017 and is also a private means of access to land north of Kirkby Thore. The Order authorises the realignment of Green Lane to the west of the existing alignment and provision of a new overbridge crossing the new A66 and reconnecting to the existing alignment of Green Lane to the south. The realignment of Green Lane would provide a replacement private means of access and a diversion of the existing public footpath.

The proposed change comprises keeping the current alignment of Green Lane and constructing the new Green Lane overbridge on the existing alignment of Green Lane. The realignment from that authorised by the Order will be beyond the current lateral LoD of Work No. 0405-9 which are 3m either side of the centreline.

Proposed Change 2 (Green Lane) therefore comprises the realignment of the centreline of Work No. 0405-9. The current LoD (as stated above) would then apply to the realigned centreline for this numbered work.

Change 3 – Long Marton

Long Marton is a local road (C3004), south-west of Long Marton village. It connects the village with the existing A66. The Order authorises the realignment of Long Marton to the south of its existing alignment and a new underpass crossing the new A66.

The proposed change comprises realigning Long Marton Road to reduce the span of the bridge, which will also reduce the amount of third party land required for its delivery. This realignment will also necessitate a reduction in the design speed to 85kph (50mph). The realigned design extends beyond the current lateral LoD of Work No. 0405-16, which are 3m either side of the centreline.

Proposed Change 3 (Long Marton) therefore comprises the realignment of the centreline of Work No. 0405-16. The current LoD (as stated above) would then apply to the realigned centreline for this numbered work.

Change 4 – Access Track at Kirkby Thore

The Order authorises the formation of a new access track providing a private means of access and a footpath connecting the realigned Cross Street with the realigned Green Lane on an alignment located to the north of Kirkby Thore and to the south of the new A66.

The proposed change comprises removing (i.e. not constructing) the new access track between the realigned Cross Street and realigned Green Lane, instead creating a new section of permissive path along the bottom of the southern embankment of the new A66 between the realigned Cross Street and Green Lane.

Proposed Change 4 (Access Track at Kirkby Thore) therefore requires the revised provision for public rights of way and private means of access between Green Lane and Cross Street to be shown on an updated version of Sheet 2 of the Rights of Way and Access Plans.

Change 5 – B6542 and Spitals Farm Access

To the south east of Spitals Farm, the existing A66 will be de-trunked and re-classified as the new B6542, which, in addition, will comprise a length of new highway (also to be classified as the B6542), running largely adjacent to the new A66. The Order authorises the stopping up and replacement of a private means of access to Spitals Farm, via an underpass beneath the new A66 and the re-classified as the B6542.

The proposed change is to the alignment of the new B6542, moving it up to 8 metres closer to the new A66. This exceeds the lateral LoD for Work No 0405-4A, which are 3 metres either side of the centreline.

Proposed Change 5 (B6542 and Spitals Farm Access) therefore comprises the realignment of the centreline for Work No. 0405-4A. The current LoD (as stated above) would then apply to the realigned centreline for that numbered work. This change would also involve a change to the replacement private means of access to Spitals Farm, insofar as the width of the underpass (beneath the de-trunked A66 / new B6542 and the new A66) would be reduced.

Change 6 – Kemplay Bank

The Order authorises a new section of A66 to be constructed to pass beneath the new Kemplay Bank junction arrangement, tying into the existing A66 alignment.

The proposed change is an amendment to Work No. 0102-1E by relocation of the centreline by 2.5 metres to the north, up to chainage 11+400. The new section will be given a new work number (Work No. 0102-1D-A). The standard LoD for linear works (as set out in article 7 of the Order) will apply to the new Work No. 0102-1D-A.

Change 7 - B6262 Combined Public Rights of Way and Private Means of Access

The Order authorises new public rights of ways (PRoW) and private means of access (PMAs) between the junction of the improved A66 with the B6262 and the Lightwater Watercourse.

The Proposed Change involves the realignment of various lengths of combined PRoW and PMA:

- on the south side of the A66, running between the B6262 and the site of the former Llama Karma Kafe, and providing access to the Countess Pillar which is located between them (comprising part of Work No. 03-7A, and both Work Nos. 03-9A and 03-9B; and
- crossing over the A66, then running along the north side of the A66 in an easterly direction (comprising part of Work No. 03-7A and part of Work No. 03-7B).

Additionally, the proposed change on the south side of the A66 comprises cycle track along the entirety of the route between the site of the former Llama Karma Kafe, the Countess Pillar and the B6262, amending the previously consented proposals which comprised mainly cycle track, but which also included a short length of footpath on the approach to the Countess Pillar.

Part of the realignment of Work No. 03-7A involves its commencement point which is proposed to be relocated 8 metres to the south of the junction of the B6262 with the A66 (in comparison with the commencement point in the previously consented proposal).

Beyond that, proposed Change 7 includes further changes to the alignment of Work No. 03-7A, which at some points exceed the lateral LoD for Work No. 03-7A, which is 3 metres either side of the centreline. Proposed Change 7 therefore includes a partial realignment of the centreline for Work No. 03-7A, to which the standard 3 metre lateral LoDs (as set out in article 7 of the Order) would subsequently apply.

Amendments of the same nature are also proposed to the alignment of the centreline of Work Nos. 03-7B, 03-9A and 03-9B.

Change 8 – Powis House Private Means of Access

The Order authorises the provision of a new footpath and private means of access (the Trout Beck Bridge south shore maintenance track) between the existing A66 and the proposed A66, and the construction of a new footpath passing beneath the Trout Beck viaduct and connecting to the existing Footpath 341/017.

The Proposed Change comprises realigning the new footpath and private means of access approximately 20 metres to the northwest. Such realignment is required in consequence of the shortening of the Trout Beck viaduct pursuant to a detailed design change (which is the subject of a separate request for authorisation, submitted to the Department). The proposed re-alignment of the footpath and private means of access

would exceed the current lateral LoD for Work No.0405-14, which is 3 metres to the north and south of the centreline of this numbered work.

Proposed Change 8 (Powis House Private Means of Access) therefore comprises the realignment of the centreline of Work No. 0405-14. The current LoD (as stated above) would then apply to the realigned centreline for this numbered work.

Pre-application engagement with relevant persons

National Highways has undertaken pre-application engagement on the proposed non-material changes with affected landowners, Westmorland & Furness Council (W&FC) and through the Community Liaison Groups established in the consultation phase of the DCO application. Each Community Liaison Group includes local councillors from affected wards, parish councillors, community representatives, businesses, and residents.

Online sessions were held with the Penrith to Kemplay, Kemplay Bank to Temple Sowerby, Kirkby Thore to Long Marton, and Appleby to Brough Community Liaison Groups in November 2024, in partnership with W&FC. In addition, the invitation was extended beyond the route of the project to all 55 parish councils that are either on the route or may be affected by the project.

An additional online session was held relating to Proposed Change 8 (Powis House Private Means of Access) in August 2025.

These events enabled attendees to see the proposed non-material changes and ask any questions of the designers. There were no specific questions raised regarding the proposed non-material changes.

As part of the non-material change application process, National Highways, as the applicant, must consult on the proposed changes to provide an opportunity for interested parties to make representations about the non-material change application. Regulation 7(2) in the Infrastructure Planning (Changes to, and Revocation of, Development Consent Orders) Regulations 2011 requires the applicant to consult 'parties who have the benefit of the made order'; parties who were notified of the application for the made order under section 56 of the PA2008; and any other party who may be directly affected by the changes proposed.

This letter is a request under regulation 7(3) of the Infrastructure Planning (Changes to, and Revocation of, Development Consent Orders) Regulations 2011. Regulation 7(3) states that an applicant need not consult a specified person or authority if they have obtained the written consent of the Secretary of State to exclude such person from the scope of the consultation on the non-material change application. The Regulation enables National Highways to seek your consent not to consult certain parties on the proposed non-material change application. Annex B sets out who we propose to consult

and who we propose not to consult. Annex A sets out our reasons for consulting certain parties, and our reasons for seeking consent not to consult certain other parties.

National Highways intends to submit its application for a non-material change to the Order by the end of summer 2025. We therefore respectfully ask that a decision is made in respect of this request for an exemption from consulting certain parties as identified in Annex B below by 25 September 2025 please.

Should you have any queries about this correspondence, the proposals or the consultation, please contact [REDACTED] at A66NTP@nationalhighways.co.uk.

Yours sincerely

[REDACTED]

[REDACTED]

DCO Project Manager for the A66 Northern Trans-Pennine Project

Annex A

National Highways undertook extensive pre-application consultation on the original application. The pre-application engagement and consultation is set out in the consultation report submitted with the application for the scheme. Following the original application being accepted for examination, National Highways publicised the accepted application and notified the relevant prescribed bodies; local authorities; those with an interest in land and a number of other consultation bodies in line with the requirements of s56 of the Planning Act 2008. Overall, 235 relevant representations were submitted in relation to the original DCO application.

In response to the previously proposed non-material change application submitted during the DCO examination, the following interested parties made representations that addressed matters local to the currently proposed non-material change application. We are intending to include all of these interested parties in our consultation on this Non-Material Amendment application. Those interested parties comprise:

- Change 1 (Cross Street)
 - Sports England
- Change 2 (Green Lane)
 - None
- Change 3 (Long Marton)
 - Barbera Lynn Iverson
 - Taylor Family
- Change 4 (Access Track at Kirkby Thore)
 - None
- Change 5 (B6542 and Spitals Farm Access)
 - Andrew Michael Addison
- Change 6 (Kemplay Bank)
 - Barrie Cheetham
 - Cumbria Constabulary
 - Cumbria County Council
- Change 7 (B6262 Combined Public Rights of Way and Private Means of Access Limits of Deviation)
 - The Ramblers, Penrith Group
 - John Harvey Slack
- Change 8 (Powis House Private Means of Access)
 - Colin Thomas Dent and Yvonne Julie Dent

The table below sets out the reasons why we will consult certain parties on the non-material change application (i.e. those parties with a 'yes' in the column headed 'to be consulted on the proposed change application' in the tables in Annex B) and why we are proposing not to consult certain parties on the non-material change application (i.e. those parties with a 'no' in the column headed 'to be consulted on the proposed change application' in the tables in Annex B).

Reasons for consulting certain parties on the proposed non-material change application	Reasons we are proposing <u>not</u> to consult certain parties on the proposed non-material change application
Prescribed consultees who may have an interest in this proposed non-material change application.	Prescribed consultees we don't consider may have an interest in this proposed non-material change application as it is unlikely to be relevant to them. (Further information in the relevant paragraph below)
Relevant statutory undertakers who may have an interest in this proposed non-material change application.	Statutory undertakers we don't consider may have an interest in this proposed non-material change application as it is unlikely to be relevant to them. (Further information in the relevant paragraph below)
Relevant local authorities who may have an interest in this proposed non-material change application.	Local authorities that are unlikely to have an interest in this proposed non-material change application as it is unlikely to be relevant to them. (Further information in the relevant paragraph below)
Those with land interests within proximity of the project who may have an interest in this proposed non-material change application.	
Other interested parties from the examination of the original DCO scheme who may have an interest in the proposed non-material change application.	
Parties having benefit of the made DCO	

In determining relevant **prescribed consultees** who may have an interest in this proposed non-material change application, we have excluded those where their area of responsibility is not relevant to the nature of the proposed change either through being geographically too far removed from the proposed non-material amendments to be directly affected by them, not responsible for the area where the change is taking place or not relevant given the nature and small scale of the proposed amendments. The same principle was adopted in respect of the non-material amendments submitted during the DCO examination in March 2023.

In determining relevant **land interests**, **interested parties** and **statutory undertakers** who may have an interest in this proposed non-material change application, we have excluded those whose interest is greater than 250 metres from the DCO Limits of

Deviation for Schemes 01-06. This is because those parties' interests are geographically too far removed from the proposed non-material amendments to be directly affected by them, given the small size and scale of the proposed amendments. The same principle was adopted in respect of the non-material amendments submitted during the DCO examination in March 2023.

In determining the **local authorities** which should be consulted, it was noted that all of the proposed non-material amendments take place solely within the administrative boundary of Westmorland & Furness Council. The changes are considered too inconsequential to be materially relevant to any other neighbouring council and therefore no others are proposed to be consulted. Similarly, it is proposed to consult only those parish councils directly affected by or immediately adjacent to a non-material amendment.

We have only included organisations and persons once in Annex B below, to avoid duplication across multiple tables, in the interests of clarity. The organisations and persons have been included in their most relevant table only.

Annex B

Prescribed Consultees

The following table lists those parties notified of the application for the made Order under section 56 of the PA2008 and explains whether it is proposed to consult or not to consult them on the non-material change application. The decision for each is based on the reasons given in Annex A.

Relevant Prescribed Consultees		Consulted on the original DCO application	To be consulted on the proposed change application
Consultee	Organisation	Yes or No	Yes or No
The Welsh Ministers	Not applicable.	No	No
The Scottish Executive	Not applicable.	No	No
The relevant Northern Ireland Department	Not applicable.	No	No
The Health and Safety Executive	HSE North West & Yorkshire & NE	Yes	Yes
The National Health Service Commissioning Board and the relevant clinical commissioning group.	North Cumbria CCG NHS Commissioning Board Authority	Yes	Yes
The National Health Service Commissioning Board and the relevant clinical commissioning group.	North Yorkshire CCG County Durham CCG	Yes	No
The relevant Health Board	Not applicable.	No	No
Natural England	Not applicable.	Yes	Yes
Historic Buildings and Monuments Commission for England	Historic England.	Yes	Yes
The relevant fire and rescue body	Fire and Rescue Authority Cumbria (NW)	Yes	Yes
The relevant fire and rescue body	Fire and Rescue Authority North Yorkshire Fire and Rescue Authority County Durham and Darlington	Yes	No

Relevant Prescribed Consultees		Consulted on the original DCO application	To be consulted on the proposed change application
Consultee	Organisation	Yes or No	Yes or No
The relevant police authority	Cumbria Police Cumbria Police and Crime Commissioner	Yes	Yes
The relevant police authority	North Yorkshire Police Traffic Officer Durham Police North Yorkshire Police and Crime Commissioner Durham Police and Crime Commissioner	Yes	No
The relevant parish council, or, where the application relates to Wales or Scotland, the relevant community council	Appleby-in-Westmorland Town Council Bolton Parish Council Brougham Parish Council Crackenthorpe Parish Council Kirkby Thore Parish Long Marton Civil Parish Penrith Town Council Temple Sowerby Parish Council Langwathby Parish Council Musgrave Parish Council Warcop Parish Council Yanwath and Eamont Bridge	Yes	Yes
The relevant parish council, or, where the application relates to Wales or Scotland, the relevant community council	Boldron Parish Council Bowes Parish Council Brough Parish Council Dacre Parish Council Dalton Parish Council East/West Layton and Carkin Parish Council Gilling with Hartforth and Sedbury Parish Council Gilmonby Parish Council Helbeck Parish Council	Yes	No

Relevant Prescribed Consultees		Consulted on the original DCO application	To be consulted on the proposed change application
Consultee	Organisation	Yes or No	Yes or No
	Hutton Magna Parish Council Middleton Tyas Parish Council Ravensworth Parish Council Rokeby/ Brignall/ Eggleston Parish Council		
The Environment Agency	The Environment Agency	Yes	Yes
The Scottish Environment Protection Agency	Not applicable.	No	No
The Equality and Human Rights Commission	Not applicable.	Yes	Yes
The Scottish Human Rights Commission	Not applicable.	No	No
AONB Conservation Boards (now National Landscapes Association)	North Pennines AONB Board (now North Pennines National Landscape Board)	Yes	Yes
Royal Commission on Ancient and Historical Monuments of Wales	Not applicable.	No	No
The Natural Resource Body for Wales (NRW)	Not applicable.	No	No
The Homes and Communities Agency	Not applicable.	Yes	Yes
The Joint Nature Conservation Committee	Not applicable.	No	No
Scottish Natural Heritage	Not applicable.	No	No
The Maritime and Coastguard Agency	Not applicable.	No	No
The Marine Management Organisation	Not applicable.	No	No
The Scottish Fisheries Protection Agency (Marine Scotland)	Not applicable.	No	No
The Civil Aviation Authority	Not applicable.	No	No
Secretary State for Transport	Not applicable.	Yes	Yes

Relevant Prescribed Consultees		Consulted on the original DCO application	To be consulted on the proposed change application
Consultee	Organisation	Yes or No	Yes or No
Integrated Transport Authorities (ITAs) and Passenger Transport Executives (PTEs)	Transport for the North	Yes	No
The relevant Highways Authorities	Westmorland and Furness Council (formerly part of Cumbria County Council)	Yes	Yes
The relevant Highways Authorities	Cumberland Council (formerly part of Cumbria County Council) Durham County Council North Yorkshire County Council	Yes	No
The relevant strategic highways company	National Highways	Yes	No
Transport for London	Not applicable	No	No
The Passengers Council	Not applicable	Yes	Yes
The Disabled Persons Transport Advisory Committee	Not applicable	Yes	Yes
The Coal Authority	Not applicable	Yes	No
The Office of Rail Regulation and approved operators	Not applicable	No	No
The Gas and Electricity Markets Authority (OFGEM)	Not applicable	No	No
The Water Services Regulations Authority (OFWAT)	Not applicable	No	No
The Water Industry Commission of Scotland	Not applicable	No	No
The relevant waste regulation authority	Not applicable.	No	No
The relevant internal drainage board	Swale & Ure Drainage Board	Yes	Yes
The Canal and River Trust	Not applicable.	Yes	Yes
Trinity House	Not applicable.	No	No

Relevant Prescribed Consultees		Consulted on the original DCO application	To be consulted on the proposed change application
Consultee	Organisation	Yes or No	Yes or No
Public Health England, an executive agency of the Department of Health	Not applicable.	Yes	Yes
The relevant local resilience forum	Cumbria Resilience Forum	Yes	Yes
The relevant local resilience forum	North Yorks LRF	Yes	No
The Crown Estate Commissioners	The Crown Estate Commissioners	Yes	No
The Forestry Commission	The Forestry Commission	Yes	No
The National Health Service Trusts	North East Ambulance Service Foundation Trust Yorkshire and the Humber Ambulance Trust	Yes	No
The National Health Service Trusts	Penrith Community Hospital North West Ambulance Trust	Yes	Yes
The Secretary of State for Defence	Not applicable.	Yes	Yes
The Office of Nuclear Regulation (the ONR)	Not applicable.	Yes	Yes

Relevant Statutory Undertakers, Utilities Companies, and Infrastructure Providers

Entity	Consult?
Electricity and Gas Suppliers	
Cadent Gas Limited	Yes
Eclipse Power Networks Ltd	No
Electricity North West Limited	Yes
Energy Assets Pipelines Ltd	No
ESP Utilities Group Limited	No
Harlaxton Energy Networks Ltd	No

Entity	Consult?
Independent Power Networks Limited	No
Indigo Pipelines Limited	No
Indigo Power Ltd	No
Last Mile Electricity Limited	No
Leep Electricity Networks Ltd	No
Leep Gas Networks Ltd	No
Mua Gas Limited	No

Entity	Consult?
National Gas Transmission plc	Yes
National Grid Electricity Transmission plc	Yes
Northern Gas Networks Limited	Yes
Northern Powergrid Limited	Yes
Npower Commercial Gas Limited	No
Optimal Power Networks Ltd	No
Quadrant Pipelines Limited	No
Scotland Gas Networks Plc	No
The Electricity Network Company Limited	Yes
UK Power Distribution Limited	No
Utility Assets Limited	No
British Gas Limited	Yes
GTC Pipelines Limited	Yes
National Grid PLC	Yes
SSE plc	No
Zayo Group UK Limited	Yes
GTC Infrastructure Limited	No
Northern Power Grid (Northeast) plc	Yes
Northern Power Grid (Yorkshire) plc	Yes
RWE Renewables UK Swindon limited	No
Water Infrastructure	
United Utilities Group plc	Yes
United Utilities Water Limited	Yes
Northumbrian Water	Yes
Yorkshire Water Limited	Yes
Telecommunications	
Fulcrum Utility Services Limited	No
Instalcom UK Limited	Yes
Mastdata Limited	Yes

Entity	Consult?
Mobile Broadband Network Limited	Yes
NRTS - Telent Technology Services Limited	No
Telefonica O2 UK Limited	Yes
Vattenfall Networks Ltd	No
Virgin Media Limited	Yes
Vodafone Limited	Yes
British Telecommunications Public Limited Company	Yes
Cornerstone Telecommunications Infrastructure Limited	Yes
EE Limited	Yes
Openreach Limited	Yes
Energis Communications Limited	Yes
Other?	
National Highways Historical Railways Estate	Yes
Network Rail Infrastructure Limited	Yes

Local Authorities	Consulted on the original DCO application	To be consulted on the proposed change application
Local Authority	Yes or No	Yes or No
Westmorland and Furness Council (formerly Eden District Council, South Lakeland District Council and Barrow Borough Council and part of Cumbria County)	Yes	Yes
Durham County Council	Yes	No
North Yorkshire Council (formerly North Yorkshire County Council, Craven District Council, Hambleton District Council, Harrogate Borough Council, Richmondshire District Council, Ryedale District Council, Scarborough Borough Council and Selby District Council)	Yes	No
Cumberland Council (formerly Allerdale Borough Council, Carlisle City Council and Copeland Borough Council and part of Cumbria County Council)	Yes	No
City of Bradford Metropolitan District Council (formerly Bradford Metropolitan District Council)	Yes	No
Darlington Borough Council	Yes	No
City of Doncaster Council (formerly Doncaster Metropolitan Borough Council)	Yes	No
East Riding of Yorkshire	Yes	No
Gateshead Council	Yes	No
Hartlepool Borough Council	Yes	No
Lake District National Park Authority	Yes	No
Lancashire County Council	Yes	No
Lancaster City Council	Yes	No
Leeds City Council	Yes	No
Middlesborough Borough Council	Yes	No
North York Moors National Park Authority	Yes	No
Northumberland County Council	Yes	No
Northumberland National Park Authority	Yes	No
Redcar and Cleveland Borough Council	Yes	No
Stockton-on-Tees Borough Council	Yes	No
Sunderland City Council	Yes	No
Wakefield Metropolitan District Council	Yes	No
York City Council	Yes	No
Yorkshire Dales National Park Authority	Yes	No
Scottish Borders Council	Yes	No
Dumfries and Galloway Council	Yes	No

The following table lists only those persons and organisations we intend to consult. We will consult all persons and organisations with land interests within a 250 metre radius of Schemes 01-06 of the DCO. For the reasons set in Annex A, it is not proposed to consult individuals or organisations outside of this radius and we have not shown them in this table for this reason.

[illegible]

[illegible]

[illegible]

disability
confident
COMMITTED

Registered office Bridge House, 1 Walnut Tree Close, Guildford GU1 4LZ
National Highways Limited registered in England and Wales number 09346363



Registered office Bridge House, 1 Walnut Tree Close, Guildford GU1 4LZ
National Highways Limited registered in England and Wales number 09346363



Registered office Bridge House, 1 Walnut Tree Close, Guildford GU1 4LZ
National Highways Limited registered in England and Wales number 09346363



Registered office Bridge House, 1 Walnut Tree Close, Guildford GU1 4LZ
National Highways Limited registered in England and Wales number 09346363



Registered office Bridge House, 1 Walnut Tree Close, Guildford GU1 4LZ
National Highways Limited registered in England and Wales number 09346363



[illegible]

Registered office Bridge House, 1 Walnut Tree Close, Guildford GU1 4LZ
National Highways Limited registered in England and Wales number 09346363



Registered office Bridge House, 1 Walnut Tree Close, Guildford GU1 4LZ
National Highways Limited registered in England and Wales number 09346363



Registered office Bridge House, 1 Walnut Tree Close, Guildford GU1 4LZ
National Highways Limited registered in England and Wales number 09346363



Entity	Consult?
Church Commissioners for England	Yes
Citicorp Trustee Company Limited	Yes
CKD Galbraith LLP	Yes
Clydesdale Bank plc	Yes
Colby Farms Limited	Yes
Confederation of Forest Industries (UK) Limited	Yes
CP Whinfell Village Limited	Yes
Crossfit Cumbria Limited	Yes
Cumberland Building Society	Yes
Daniel Thwaites plc	Yes
Eamont Competition Horses Limited	Yes
Eden Housing Association Limited	Yes
Eden Valley Railway Trust	Yes
G Calvert & Sons	Yes
Glendale Grounds Management Limited	Yes
Handelsbanken plc	Yes
Happy Hooves RDA	Yes
HSBC Bank plc	Yes
HSBC Corporate Trustee Company (UK) Limited	Yes
HSBC UK Bank plc	Yes
JB & GE Luck	Yes
JIW Properties Limited	Yes
Kier Transportation Limited	Yes
Kirkby Thore School	Yes
KTEE Limited	Yes
L.E.T. Nominees 1 Limited as Executor of The Right Honourable Hugh Clayton Lowther Eighth Earl of Lonsdale	Yes
L.E.T. Nominees 2 Limited	Yes
Lloyds Bank plc	Yes
Lowther Estates Limited	Yes
Lyle Farming Company	Yes
M. J. Close & Son Limited	Yes
Maple Bridge Corporation Limited	Yes
National Westminster Bank plc	Yes

Entity	Consult?
Newcastle Building Society	Yes
North Cumbria Integrated Care NHS Foundation Trust	Yes
NP & MD Lancaster	Yes
Optimum Fire & Interiors Limited	Yes
O'Reilly Wealth Management Limited	Yes
Penrith & District Farmers Mart LLP	Yes
Penrith Angling Association	Yes
Penrith Properties Limited	Yes
Pensioner Trustees Limited	Yes
Peter Sorton & Associates Limited	Yes
Pure Retirement Limited	Yes
R Brennand Engineering Limited	Yes
Railway Paths Limited	Yes
Saddle Skedaddle Limited	Yes
Santander UK plc	Yes
Shell Chemicals U.K. Limited	Yes
Shell Group Limited	Yes
Skirsgill Dental Surgery	Yes
Story Contracting Limited	Yes
Summerfields Books 2007 Limited	Yes
Taylor & Braithwaite Limited	Yes
Telereal Securitised Property Trustee 1 Limited	Yes
The Agricultural Mortgage Corporation plc	Yes
The Churches Conservation Trust	Yes
The Governing Body of Ullswater Community College	Yes
The Kingdom Hall Trust	Yes
The Law Debenture Trust Corporation plc	Yes
The Mortgage Business plc	Yes
The Official Custodian for Charities	Yes
The Penrith Farmers and Kidd's plc	Yes
The Riverside Group Limited	Yes

Entity	Consult?
Trustees of St Anne's Hospital	Yes
Ullswater Road Body Repairs Limited	Yes
Urbaser Limited	Yes
Voluntary Action Cumbria	Yes

Entity	Consult?
Wearmouth Plant Hire Limited	Yes
Xafinity Pension Trustees Limited as Trustees of the XPS SIPP Re David Hayllar 102620-1 and Christine Hayllar 102620-2	Yes

Land interest organisations not being consulted

The following table lists those organisations we do not intend to consult that were previously consulted. The decision for this is based on the reasons given in Annex A. This table does not include individual land interests who we do not intend to consult.

Entity	Consult?
Land interest organisations	
Coal Authority	No
Confederation of Passenger Transport	No
County Durham and Darlington Fire and Rescue Service	No
Cumbria Fire & Rescue Service	No
Cumbria Prepared	No
Disabled persons transport advisory committee	No
Durham Constabulary	No
Equality and Human Rights Commission	No
Forestry Commission	No
Health and Safety Executive - Yorkshire and North East	No
Historic England Limited	No
Homes England	No
Humber and North Yorkshire Health and Care Partnership	No

Entity	Consult?
Hyslop & Atkinson Limited	No
NHS County Durham Clinical Commissioning Group	No
North Yorkshire Fire & Rescue Service	No
North Yorkshire Local Resilience Forum	No
Office for Nuclear Regulation	No
Ravensworth Nurseries Limited	No
Royal Mail Group Limited	No
Swale & Ure Drainage Board - Yorkshire RFCC Area	No
The Crown Estate Commissioners	No
Transport for the North	No
UK Health Security Agency	No
Vodafone Plant Enquiries	No

Other interested parties from the examination of the original DCO scheme who raised matters that are relevant to the proposed non-material change proposals

During the DCO examination, the below interested parties made representations that addressed matters local to the previously proposed non-material change application. We are intending to include these same parties in our consultation on this forthcoming Non-Material Amendment application.

Name
Sports England

Cumbria Constabulary	
Cumbria County Council	
The Ramblers, Penrith Group	

Parties having benefit of the made DCO

National Highways Limited (company number 09346363) whose registered office is Bridge House, 1 Walnut Tree Close, Guildford, Surrey, GU1 4LZ is the only party having the benefit of the made DCO. As the applicant, it is not intended to consult National Highways on the proposed change application.

Letter from the Secretary of State confirming who must be consulted on this Non-Material Change Application



[Redacted]
National Highways
9th Floor
Piccadilly Gate
Store Street
Manchester
M1 2WD

[Redacted]
Joint Head of the Transport Infrastructure
Planning Unit
Department for Transport
Great Minster House
33 Horseferry Road
London
SW1P 4DR

Email: transportinfrastructure@dft.gov.uk
Web Site: www.dft.gov.uk

5 November 2025

Dear [Redacted]

A66 Northern Trans-Pennine Development Consent Order 2024 (SI 2024 No. 360) – Proposed Non-Material Change Application

Regulation 7(3) of the Infrastructure Planning (Changes to, and Revocation of, Development Consent Orders) Regulations 2011 – Written consent from the Secretary of State for not consulting a person or authority

Thank you for your correspondence dated 28 August 2025 requesting consent to reduce the number of parties to be consulted on the proposed non-material change application for the A66 Northern Trans-Pennine Development Consent Order 2024 ("the Order").

The Secretary of State has considered this request under regulation 7(3) of the Infrastructure Planning (Changes to, and Revocation of, Development Consent Orders) Regulations 2011 (as amended) ("the 2011 Regulations"). The Secretary of State agrees that the consultation approach set out in your submission, focusing on parties likely to have an interest in or be directly affected by the proposed changes, is proportionate and reasonable. The Secretary of State accepts that those parties identified for exclusion are either geographically too remote, not materially affected, or otherwise outside the scope of the proposed amendments.

Accordingly, the Secretary of State gives written consent, to the extent set out above, under regulation 7(3) of the 2011 Regulations. In reaching this decision, the Secretary of State notes that while those persons not proposed to be consulted will not be contacted directly, the application will be publicised in accordance with regulation 20 of the 2011 Regulations and will be available on the Planning Inspectorate's National Infrastructure Planning website. The Secretary of State also notes your confirmation that all relevant statutory authorities will still be consulted.

Please note that this consent should not be taken as indicating approval of any aspects of the proposed changes to the Order, which will fall to the Secretary of State for consideration and determination, or whether the proposed changes will ultimately be regarded as material or not.

Yours sincerely,

[Redacted Signature]

Annex B. List of Certified Documents to be amended by Change Application

The documents to be certified by the Secretary of State in relation to the Order are presented in a table in Schedule 10 to the Order ('Certified Documents'). This table comprises four columns:

- Column 1: (1) Documents e.g. *Works Plans*.
- Column 2: (2) Description e.g. *Works Plans Scheme 0102: M6 Junction 40 to Kemplay Bank*
- Column 3: (3) Reference e.g. *5.16 (Rev 2)*; and
- Column 4: (4) Date Submitted e.g. *9/5/2023*

Each Scheme-specific drawing pack comprises a series of drawing sheets with their own drawing numbers. Neither the sheet numbers nor drawing numbers are listed in Schedule 10 to the Order.

The Proposed Changes require the submission of amended individual drawing sheets to replace sheets from the certified documents listed in Schedule 10 to the Order. A summary of the amendments to Certified Documents and a table listing all amended drawing sheets within the Scheme-specific drawing packs are set out below.

The amended drawing sheets submitted with this Change Application show the geographic extents of the Proposed Changes. Although the eight Proposed Changes are localised and small in scale, they affect a relatively large number of drawing sheets with 56 drawing sheets to be amended due to the linear nature of the Project and the large number of drawing sheets within each Scheme specific pack of Certified drawings.

Changes are required to individual sheets within the following Certified Documents:

- Engineering Section Drawings: Cross Sections
- Engineering Section Drawings: Plan and Profiles
- Rights of Way and Access Plans
- Works Plans
- Classification of Roads Plans
- Traffic Regulation Measures (Speed Limits) Plans
- Traffic Regulation Measures (Clearways and Prohibitions) Plans
- De-trunking Plans

Changes to these drawings affect the Scheme-specific drawing packs:

- Scheme 0102: M6 Junction 40 to Kemplay Bank

- Scheme 03: Penrith to Temple Sowerby
- Scheme 0405: Temple Sowerby to Appleby

The following table lists all the individual drawing sheets accompanying this Change Application, along with the corresponding drawing sheet of the Certified Documents that will be replaced. All other Certified Documents set out in Schedule 10 to the Order as made remain unaltered.

A new drawing pack will be issued for the 'Classification of Roads Plans' for Scheme 0405 Temple Sowerby to Appleby, as both sheets have been updated as a result of the Proposed Changes.

If this Change Application is approved, the affected replacement drawing sheets will be issued as a new revision as per the revised drawings note in Annex B.

Drawing Certified by Schedule 10 of the Order								Revised Drawings for Non-Material Change Application	
(1) Document	Proposed Change Number	(2) Description	(3) Reference	(4) Date submitted	Sheet Number	Drawing Number	Drawing Revision Number	Drawing Number	Drawing Revision Number
Classification of Roads	1, 2, 3, 4 and 5	Classification of Roads Plans Scheme 0405 Temple Sowerby to Appleby	5.20	21/06/2022	1	HE565627-AMY-HAC-S0405-DR-CH-301301	C01	HE615324-BBA-HAC-004_AL_SCHEM-DR-CH-301301	P01
					2	HE565627-AMY-HAC-S0405-DR-CH-301302	C01	HE615324-BBA-HAC-004_AL_SCHEM-DR-CH-301302	P02
De-Trunking Plans	1 and 5	De-Trunking Plans Scheme 0405: Temple Sowerby to Appleby	5.21	21/06/2022	1	HE565627-AMY-HAC-S0405-DR-CH-301701	C01	HE615324-BBA-HAC-004_AL_SCHEM-DR-CH-301701	P02
	1, 2 and 4				2	HE565627-AMY-HAC-S0405-DR-CH-301702	C01	HE615324-BBA-HAC-004_AL_SCHEM-DR-CH-301702	P02
	8				3	HE565627-AMY-HAC-S0405-DR-CH-301703	C01	HE615324-BBA-HAC-004_AL_SCHEM-DR-CH-301703	P01
	3, and 8				4	HE565627-AMY-HAC-S0405-DR-CH-301704	C01	HE615324-BBA-HAC-004_AL_SCHEM-DR-CH-301704	P02
Engineering Section Drawings: Cross Sections	7	Engineering Section Drawings: Cross Sections Scheme 03: Penrith to Temple Sowerby	5.18 (Rev 2)	09/05/2023	1	HE565627-AMY-HAC-S03-DR-CH-201101	C02	HE615323-JAC-HAC-03A_AL_SCHEM-DR-CH-201101	P03
	1 and 5	Engineering Section Drawings (Cross Sections) Scheme 0405 Temple Sowerby to Appleby	5.18 (Rev 2)	09/05/2023	1	HE565627-AMY-HAC-S0405-DR-CH-301101	C01	HE615324-BBA-HAC-004_AL_SCHEM-DR-CH-301101	P01
	4				3	HE565627-AMY-HAC-S0405-DR-CH-301110	C01	HE615324-BBA-HAC-004_AL_SCHEM-DR-CH-301103	P01
	3				5	HE565627-AMY-HAC-S0405-DR-CH-301105	C01	HE615324-BBA-HAC-004_AL_SCHEM-DR-CH-301105	P02
Engineering Section Drawings: Plan and Profiles	6	Engineering Section Drawings: Plan and Profiles Scheme 0102: M6 Junction 40 to Kemplay Bank	5.17 (Rev 2)	09/05/2023	2	HE565627-AMY-HAC-S0102-DR-CH-101101	C02	HE615322-KIE-HAC-002_AL_SCHEM-DR-CH-101001	P01
	7	Engineering Section Drawings: Plan and Profiles Scheme 03: Penrith to Temple Sowerby	5.17 (Rev 2)	09/05/2023	1	HE565627-AMY-HAC-S03-DR-CH-201001	C02	HE615323-JAC-HAC-03A_AL_SCHEM-DR-CH-201001	P01
	1 and 5	Engineering Section Drawings: Plan and Profiles Scheme 0405: Temple Sowerby to Appleby	5.17 (Rev 2)	09/05/2023	1	HE565627-AMY-HAC-S0405-DR-CH-301001	C02	HE615324-BBA-HAC-004_AL_SCHEM-DR-CH-301001	P02
	1, 2 and 4				2	HE565627-AMY-HAC-S0405-DR-CH-301002	C02	HE615324-BBA-HAC-004_AL_SCHEM-DR-CH-301002	P02
	3 and 8				4	HE565627-AMY-HAC-S0405-DR-CH-301004	C01	HE615324-BBA-HAC-004_AL_SCHEM-DR-CH-301004	P02
	3				5	HE565627-AMY-HAC-S0405-DR-CH-301005	C02	HE615324-BBA-HAC-004_AL_SCHEM-DR-CH-301005	P01
	1 and 5				8	HE565627-AMY-HAC-S0405-DR-CH-301008	C02	HE615324-BBA-HAC-004_AL_SCHEM-DR-CH-301008	P02
	1, 2 and 4				9	HE565627-AMY-HAC-S0405-DR-CH-301009	C02	HE615324-BBA-HAC-004_AL_SCHEM-DR-CH-301009	P02
	3 and 8				11	HE565627-AMY-HAC-S0405-DR-CH-301011	C01	HE615324-BBA-HAC-004_AL_SCHEM-DR-CH-301011	P02
	3				12	HE565627-AMY-HAC-S0405-DR-CH-301012	C02	HE615324-BBA-HAC-004_AL_SCHEM-DR-CH-301012	P02
	5				15	HE565627-AMY-HAC-S0405-DR-CH-301016	C01	HE615324-BBA-HAC-004_AL_SCHEM-DR-CH-301016	P02
	5				16	HE565627-AMY-HAC-S0405-DR-CH-301017	C01	HE615324-BBA-HAC-004_AL_SCHEM-DR-CH-301017	P01
	1, 2 and 4				17	HE565627-AMY-HAC-S0405-DR-CH-301018	C02	HE615324-BBA-HAC-004_AL_SCHEM-DR-CH-301018	P02
	1				18	HE565627-AMY-HAC-S0405-DR-CH-301019	C02	HE615324-BBA-HAC-004_AL_SCHEM-DR-CH-301019	P01
	1, 2 and 4				19	HE565627-AMY-HAC-S0405-DR-CH-301020	C02	HE615324-BBA-HAC-004_AL_SCHEM-DR-CH-301020	P02
	2				20	HE565627-AMY-HAC-S0405-DR-CH-301021	C01	HE615324-BBA-HAC-004_AL_SCHEM-DR-CH-301021	P01

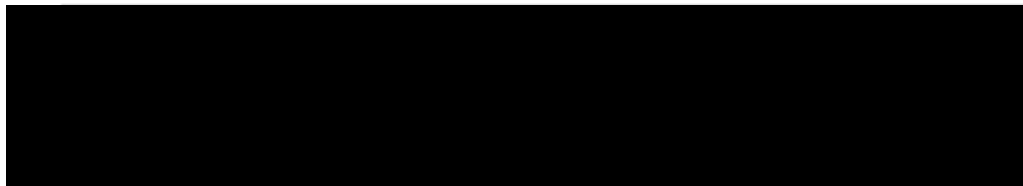
Drawing Certified by Schedule 10 of the Order								Revised Drawings for Non-Material Change Application	
(1) Document	Proposed Change Number	(2) Description	(3) Reference	(4) Date submitted	Sheet Number	Drawing Number	Drawing Revision Number	Drawing Number	Drawing Revision Number
	3				22	HE565627-AMY-HAC-S0405-DR-CH-301023	C01	HE615324-BBA-HAC-004_AL_SCHEM-DR-CH-301023	P01
	3				23	HE565627-AMY-HAC-S0405-DR-CH-301024	C01	HE615324-BBA-HAC-004_AL_SCHEM-DR-CH-301024	P01
Rights of Way and Access Plans	7	Rights of Way and Access Plans Scheme 03: Penrith to Temple Sowerby	5.19 (Rev 3)	16/05/2023	1	HE565627-AMY-HAC-S03-DR-CH-201401	C02	HE615323-JAC-HAC-03A_AL_SCHEM-DR-CH-201401	P03
	1 and 5	Rights of Way and Access Plans Scheme 0405: Temple Sowerby to Appleby	5.19	21/06/2022	1	HE565627-AMY-HAC-S0405-DR-CH-301401	C01	HE615324-BBA-HAC-004_AL_SCHEM-DR-CH-301401	P02
	1, 2 and 4				2	HE565627-AMY-HAC-S0405-DR-CH-301402	C01	HE615324-BBA-HAC-004_AL_SCHEM-DR-CH-301402	P02
	2, and 4				3	HE565627-AMY-HAC-S0405-DR-CH-301403	C01	HE615324-BBA-HAC-004_AL_SCHEM-DR-CH-301403	P02
	8				4	HE565627-AMY-HAC-S0405-DR-CH-301404	C02	HE615324-BBA-HAC-004_AL_SCHEM-DR-CH-301404	P01
	3, and 8				5	HE565627-AMY-HAC-S0405-DR-CH-301405	C01	HE615324-BBA-HAC-004_AL_SCHEM-DR-CH-301405	P02
Traffic Regulation Measures (Clearways and Prohibitions) Plans	7	Traffic Regulation Measures (Clearways and Prohibitions) Plans Scheme 03: Penrith to Temple Sowerby	5.22 (Rev 2)	09/05/2023	1	HE565627-AMY-HAC-S03-DR-CH-201601	C02	HE615323-JAC-HAC-03A_AL_SCHEM-DR-CH-201601	P01
	1 and 5	Traffic Regulation Measures (Clearways and Prohibitions) Plans Scheme 0405: Temple Sowerby to Appleby	5.22	21/06/2022	1	HE565627-AMY-HAC-S0405-DR-CH-301601	C01	HE615324-BBA-HAC-004_AL_SCHEM-DR-CH-301601	P02
	1, 2 and 4				2	HE565627-AMY-HAC-S0405-DR-CH-301602	C01	HE615324-BBA-HAC-004_AL_SCHEM-DR-CH-301602	P02
	2 and 4				3	HE565627-AMY-HAC-S0405-DR-CH-301603	C01	HE615324-BBA-HAC-004_AL_SCHEM-DR-CH-301603	P02
	8				4	HE565627-AMY-HAC-S0405-DR-CH-301604	C01	HE615324-BBA-HAC-004_AL_SCHEM-DR-CH-301604	P01
	3 and 8				5	HE565627-AMY-HAC-S0405-DR-CH-301605	C01	HE615324-BBA-HAC-004_AL_SCHEM-DR-CH-301605	P02
Traffic Regulation Measures (Speed Limits) Plans	7	Traffic Regulation Measures (Speed Limits) Plans Scheme 03: Penrith to Temple Sowerby	5.23 (Rev 2)	09/05/2023	1	HE565627-AMY-HAC-S03-DR-CH-201501	C02	HE615323-JAC-HAC-03A_AL_SCHEM-DR-CH-201501	P01
	1 and 5	Traffic Regulation Measures (Speed Limits) Plans Scheme 0405: Temple Sowerby to Appleby	5.23 (Rev 2)	09/05/2023	1	HE565627-AMY-HAC-S0405-DR-CH-301501	C01	HE615324-BBA-HAC-004_AL_SCHEM-DR-CH-301501	P02
	1, 2 and 4				2	HE565627-AMY-HAC-S0405-DR-CH-301502	C03	HE615324-BBA-HAC-004_AL_SCHEM-DR-CH-301502	P02
	2 and 4				3	HE565627-AMY-HAC-S0405-DR-CH-301503	C02	HE615324-BBA-HAC-004_AL_SCHEM-DR-CH-301503	P02
	8				4	HE565627-AMY-HAC-S0405-DR-CH-301504	C02	HE615324-BBA-HAC-004_AL_SCHEM-DR-CH-301504	P01
	3 and 8				5	HE565627-AMY-HAC-S0405-DR-CH-301505	C01	HE615324-BBA-HAC-004_AL_SCHEM-DR-CH-301505	P02
Works Plans	6	Works Plans Scheme 0102: M6 Junction 40 to Kemplay Bank	5.16 (Rev 2)	09/05/2023	2	HE565627-AMY-HAC-S0102-DR-CH-101201	C02	HE615322-KIE-HAC-002_AL_SCHEM-DR-CH-101201	P02
	7	Works Plans Scheme 03: Penrith to Temple Sowerby	5.16 (Rev 2)	09/05/2023	1	HE565627-AMY-HAC-S03-DR-CH-201201	C02	HE615323-JAC-HAC-03A_AL_SCHEM-DR-CH-201201	P01
	5	Works Plans Scheme 0405: Temple Sowerby to Appleby	5.16 (Rev 2)	09/05/2023	1	HE565627-AMY-HAC-S0405-DR-CH-301201	C02	HE615324-BBA-HAC-004_AL_SCHEM-DR-CH-301201	P02
	1, 2 and 4				2	HE565627-AMY-HAC-S0405-DR-CH-301202	C02	HE615324-BBA-HAC-004_AL_SCHEM-DR-CH-301202	P02
	1, 2 and 4				3	HE565627-AMY-HAC-S0405-DR-CH-301203	C02	HE615324-BBA-HAC-004_AL_SCHEM-DR-CH-301203	P02
	8				4	HE565627-AMY-HAC-S0405-DR-CH-301204	C03	HE615324-BBA-HAC-004_AL_SCHEM-DR-CH-301204	P01
	3 and 8				5	HE565627-AMY-HAC-S0405-DR-CH-301205	C01	HE615324-BBA-HAC-004_AL_SCHEM-DR-CH-301205	P02

Annex C. Consultation Evidence

Correspondence with Westmorland and Furness Council (WaFC) in relation to Proposed Change 1 Cross Street, Proposed Change 2 Green Lane, Proposed Change 3 Long Marton, Proposed Change 4 Access Track at Kirkby Thore, Proposed Change 5 Spitals Farm Access and Proposed Change 8 Powis House Private Means of Access

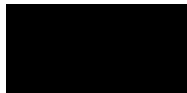
 Outlook

FW: A66 Scheme 4/5 Proposed DCO Changes



Gents,

Confirmation below that Wetmoreland and Furness Council are content with all our DCO changes.



BEng CEng MICE

Senior Project Manager
Transportation
AtkinsRéalis



@WestmorlandandFurness.gov.uk>

Sent: 02 December 2024 09:21



Subject: RE: A66 Scheme 4/5 Proposed DCO Changes

Hi 

Thank you for sharing the plans showing proposed changes on Scheme 04/05.

I can confirm that Westmorland and Furness Council has no objection to the proposed changes.

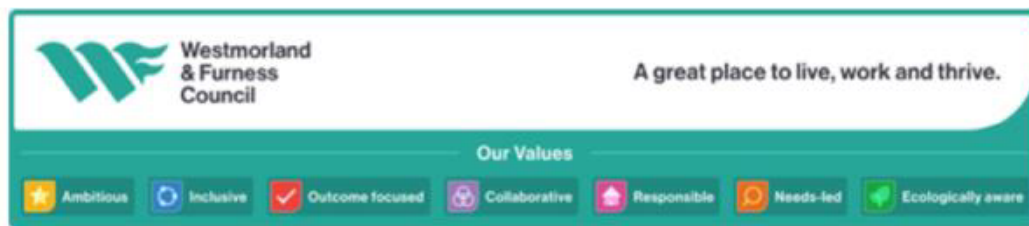
Kind regards



Senior Programme Manager (Major and Strategic Developments)
Sustainable Transport and Highways | Thriving Places
Westmorland and Furness Council | South Lakeland House | Lowther Street | Kendal |
LA9 4DQ

Mobile 

[westmorlandandfurness.gov.uk](https://www.westmorlandandfurness.gov.uk)



Please be aware that I work flexible hours, so whilst this is a convenient time for me to send this message I do not expect a response outside normal working hours.



Apologies for the length of this email but I had a bit to fit in. I think it is self-explanatory but after you have had chance to digest if you would like a meeting to discuss I'm happy to organise.

As discussed last week I attach drawings of the post DCO changes we intend to submit to the Secretary of State. I've been asked to send this email to you so you can review and let me know that you are aware and supportive of the changes. This isn't the formal consultation which will happen when they are submitted to the SoS.

These are being progressed as non-material changes to the DCO which means they have no material change to the conclusions of the Environmental Statement submitted as part of the DCO.

They hopefully aren't be new to you as they are either a result of consultation with yourselves or landowners which we have discussed and have been included in the design previously submitted for review. I have attached drawings and below is a brief description of the change and why we are proposing it.

Cross Street – (HE615324-BBA-HSR-004_UN6_SCHEM-SK-CH-000002)

The alignment of Cross Street was altered to follow the design criteria or the Rural Roads Design Guide. We narrowed the alignment and introduced horizontal curvature to better match the existing geometry and reduce the speed of traffic both approaching and leaving Kirkby Thore. This took the design outside the Limits of Deviation of the DCO.

The change is evidenced by the design speed note we submitted and subsequently revised following WSP and W&F comment.

Long Marton – (HE615324-BBA-HSR-004_C3063_SCHEM-SK-CH-000001)

This alignment of Long Marton was altered following consultation with W&F to reduce the width to match the Rural Roads Design Guide and alter the horizontal alignment to a lower and more preferable design speed. This was to better match the existing highway beyond the tie in. It also has the environmental benefit of reducing the land take for the scheme.

The change is evidenced by the design speed note we submitted and subsequently revised following WSP and W&F comment.

Access Track at Cross Street – (HE615324-BBA-HSR-004_UN6_SCHEM-SK-CH-000002)

This change followed landowner consultation. The private access track shown in the DCO was not wanted by the landowner, they asked for it to be removed. This would allow more field to be available for future use.

The footpath that shared this route was relocated to the field boundary as shown on the sketch. This retained the nmu route from Cross Street to the proposed overbridge.

This change was included in the design model submitted to W&F for review earlier this year.

Green Lane Bridge – (HE615324-BBA-HSR-004_C3065_SCHEM-SK-CH-000001)

This change was also implemented following landowner consultation. Simply the proposed overbridge has been moved to the east beyond the limits of deviation. This was to place it closer to the existing farm PMA and maximise the amount of land to the west of the alignment that could be farmed in the future. It had the additional benefit of a minor shortening of the bridge.

The bridge carries a PMA and footpath over the proposed A66. This change was included in the design model submitted to W&F for review earlier this year.

Spitals Farm Underpass and Moorland Road – (HE615324-BBA-HSR-004_B6542A_SCHEM-SK-CH-000001)

These two changes are to the alignment from the new roundabout to the tie with the detrunked A66.

The alignment was moved closer to the A66 mainline at Spitals Farm Underpass. Sufficient separation was retained so full width verges are provided for both the A66 and the sideroad. This had several benefits, it improved the visibility from the caravan park and removed a departure from standard for their exit. It reduced the length of the underpass by around 8m and subsequently reduced the land take of the scheme both of these reductions have significant environmental benefits to the scheme.

At the roundabout end of the alignment the road was lowered by 2m. This allowed the roundabout to be constructed at similar levels to the existing road which would ease construction phasing and traffic management arrangements meaning it is easier and safer to construct.

The change is evidenced by the design speed note we submitted and subsequently revised following WSP and W&F comment.

Removal of Green Strips on Bridges – (Bat Commuting Corridor Section)

The attached sketch was on the Sharepoint Site where we share documents for the Thursday DIP and W&F meetings. However, I can only find a brief mention of the green strips in the minutes. What I believe we discussed was that we had to gain agreement to any change from Natural England for the removal with suitable alternative bat route proposals.

The reason for removal of the strips is that our environmental experts consider strip planting on bridges to offer limited benefits with regard to guiding bats. Additionally including planting on bridge would increase size of structure and increase maintenance considerations.

So we have increased the planting either side of the highway to encourage the flight of bats over the road as shown on the attached sketch. I have attached email correspondence with Natural England to evidence that they agree with our proposed change.

Regards,

[Redacted]

[Redacted]

CEng MICE

Principal Engineer
Transportation
AtkinsRéalis

[Redacted]

Manchester, M1 4HN, GB

 AtkinsRéalis

An industry-first #HackaFuture

[FIND OUT MORE](#)



[Podcasts](#) [Beyond Engineering](#) [Careers](#)



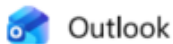
At AtkinsRéalis, we work flexible hours around the world. Although I have sent this email at a time convenient for me, I don't expect you to respond until it works for you.

NOTICE – This email message and any attachments may contain information or material that is confidential, privileged, and/or subject to copyright or other rights. Any unauthorized viewing, disclosure, retransmission, dissemination, or other use of or reliance on this message or anything contained therein is strictly prohibited and may be unlawful. If you believe you may have received this message in error, kindly inform the sender by return email and delete this message from your system. Thank you.

WARNING:

*******WARNING: Who is this email really from - ? It may not be the person you think it is! Check from internal sources before responding! Don't be scammed!*******

Email attachments may contain malicious and harmful software. If this email is unsolicited and contains an attachment DO NOT open the attachment and advise the ICT Service Desk immediately. Never open an attachment or click on a link within an email if you are not expecting it or it looks suspicious. Do not forward chain emails.



FW: ROW Change at Troutbeck

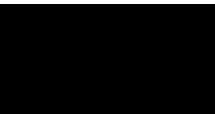
From: A66 NTP <A66NTP@cumbria.gov.uk>**Sent:** 05 December 2025 13:47
Subject: RE: ROW Change at TroutbeckHi 

Thank you for your emails regarding the proposed DCO change to the public right of way at the proposed Troutbeck viaduct.

I appreciate that the landowner is in agreement and I can confirm that the Council has no objection in principle to the proposed change.

My only caveat is to ask whether there were any representations made about the diversion of this particular right of way at the DCO Examination stage? If there were, I suggest that it would be appropriate to inform that party and provide them with the opportunity to comment on the change.

Kind regards


Senior Programme Manager (Major and Strategic Development)
Sustainable Transport and Highways | Thriving Places
Westmorland and Furness Council | South Lakeland House | Lowther Street | Kendal | LA9 4DQMobile: [westmorlandandfurness.gov.uk](https://www.westmorlandandfurness.gov.uk)

Please be aware that I work flexible hours, so whilst this is a convenient time for me to send this message I do not expect a response outside normal working hours.



Hi [REDACTED]

We briefly spoke about this on yesterday's Progressive Assurance Call, we are due to submit our non-material change application next week and we just want an email confirming that Westmorland and Furness Council are content with the change to the public right of way that will pass under Troutbeck Viaduct. We have consulted with the landowner that this affects and they are content with the change.

By the looks of it, it was sent earlier this week to the inbox, however I'm not sure if the @cumbria.gov.uk emails still work?

Please could you confirm whether the council are content with this change so that we can include the email in our application as evidence?

Kind regards

[REDACTED]
Project Manager: DCO & Schemes 9 and 11 on A66

A66 Northern Trans-Pennine Project
Regional Investment Programme North
National Highways | 9th Floor | Piccadilly Gate | Store Street | Manchester | M1 2WD
[REDACTED]

Web: <https://nationalhighways.co.uk/our-roads/a66-northern-trans-pennine/>

Twitter: [www.twitter.com/A66NTP](https://twitter.com/A66NTP)

Facebook: <https://www.facebook.com/A66-Northern-Trans-Pennine-project-101479452278722/>

From: [REDACTED] <[REDACTED]@atkinsrealis.com>
Sent: 05 December 2025 10:32
To: [REDACTED] <[REDACTED]@nationalhighways.co.uk>
Subject: FW: ROW Change at Troutbeck

[REDACTED] CEng MICE
Chief Engineer
Transportation
[REDACTED]
AtkinsRéalis

AtkinsRéalis - Baseline / Référence

[REDACTED]
A66 NTP <a66ntp@cumbria.gov.uk>
Subject: ROW Change at Troutbeck

H [REDACTED]

I'm emailing following out brief call earlier. Attached is a drawing that shows the change in public right of way that results from the shortening of Troutbeck Viaduct. We discussed this a while ago but I would like your feedback on the proposal please as this is a DCO change.

It does make the route slightly longer but as previously discussed there is the more direct route (for users travelling to Long Marton) to use the facility along the detrunked A66 and then the new Long Marton Road.

If you need anything else please let me know.

Thanks,

[REDACTED]
[REDACTED] CEng MICE
Chief Engineer
Transportation
[REDACTED]
AtkinsRéalis

Securing the future by empowering
those who protect us

JOIN US AT DSEI 2025 »

Image provided by:
UK MOD © Crown copyright 2025


[Podcasts](#) [Beyond Engineering](#) [Careers](#)



At AtkinsRéalis, we work flexible hours around the world. Although I have sent this email at a time convenient for me, I don't expect you to respond until it works for you.

 AtkinsRéalis

Correspondence with WaFC in relation to Proposed Change 6 Kemplay Bank

 Outlook

FW: Kemplay DCO change consultation with WMF

Subject: RE: Kemplay DCO change consultation with WMF

Hi [REDACTED]

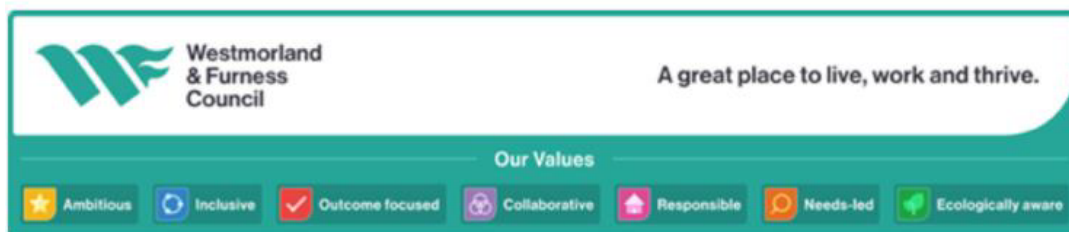
I can confirm that Westmorland and Furness Council has no objection to the proposed change of centre-line as set out in your email below.

Kind regards

[REDACTED]
Senior Programme Manager (Major and Strategic Developments)
Sustainable Transport and Highways | Thriving Places
Westmorland and Furness Council | South Lakeland House | Lowther Street | Kendal |
LA9 4DQ

Mobile: [REDACTED]

[westmorlandandfurness.gov.uk](https://www.westmorlandandfurness.gov.uk)



Please be aware that I work flexible hours, so whilst this is a convenient time for me to send this message I do not expect a response outside normal working hours.



[REDACTED]

Subject: Kemplay DCO change consultation with WMF

Importance: High

Hi [REDACTED]

Thank you very much for your time earlier today to go through the presentation of the DCO change on Scheme 02 Kemplay Bank.

I've attached the slide that I've shared on screen this morning, which summarises the DCO centreline change right after section/work no. 0102-1D, moving the DCO centreline in a section of work no. 0102-1E and maintaining the current Limits of Deviation of 3m in that section.

As part of the DCO change application process, appreciate if you can confirm that WMF has been consulted on this and confirm you had no issues with this change application.

Thank you

Kind Regards,



Follow us on:

[REDACTED] MEng CEng MICE

Senior Project Manager - Major Projects

[LinkedIn](#)
[Twitter](#)

Kier Transportation

M: [REDACTED]
E: [REDACTED]@kier.co.uk

The McLaren Building, 46 The Priory, Queensway
Birmingham B4 7LR

My working hours are: 08.00 - 17.30 (Monday - Thursday) / 08.00 - 12.00 (Friday)

Kier Transportation Limited | Registered in England No. 5606089

Registered office: 2nd Floor, Optimum House, Clippers Quay, Salford, M50 3XP

This email is sent on behalf of Kier Group. This email and any attachments are confidential and intended solely for the use of the individual to whom they are addressed. If you are not the addressee, please do not use or publish its contents; please notify the sender immediately that you have received the email and then delete it. Contracts cannot be concluded with us nor services effected by email. Emails are not secure and may contain viruses, you are advised to scan all messages for viruses with your own anti-virus programme. Kier Group may monitor emails. If you are the intended recipient of the email, upon receipt it is your responsibility to ensure that you comply with data protection laws that govern its processing and Kier Group policy where appropriate.

WARNING:

*******WARNING: Who is this email really from - ? It may not be the person you think it is!
Check from internal sources before responding! Don't be scammed!*******

Email attachments may contain malicious and harmful software. If this email is unsolicited and contains an attachment DO NOT open the attachment and advise the ICT Service Desk immediately. Never open an attachment or click on a link within an email if you are not expecting it or it looks suspicious. Do not forward chain emails.

Correspondence with WaFC in relation to Proposed Change 7 B6262 Combined Public Rights of Way and Private Means of Access

 Outlook

RE: A66 NTP Scheme 3a

From A66 NTP <A66NTP@westmorlandandfurness.gov.uk>

Date Wed 13/08/2025 1:16 PM

[WARNING] This email is from outside of the organisation. Do not open attachments or links if you are unsure of the source.

[Block sender](#) | [Report](#)

Thank you for your emails regarding the minor DCO changes to the PMA at the western end of Scheme 3A.

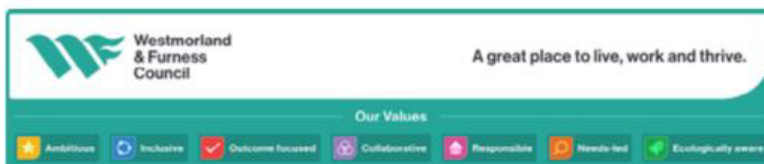
We are content with the reasons for the proposed changes to the alignment and support them. However, we still maintain our concern regarding the PMA status of the road connecting the B6262 to the Llama Karma Kafe and certainty regarding public vehicular access and car parking for the Countess Pillar.

Kind regards

[Redacted]
[Redacted] (Major and Strategic Developments)
[Redacted] Highways | Thriving Places
[Redacted] Council | South Lakeland House | Lowther Street | Kendal | LA9 4DQ

[westmorlandandfurness.gov.uk](mailto:A66NTP@westmorlandandfurness.gov.uk)

Upcoming leave: 22nd and 28th August and 8th to 12th September 2025



Please be aware that I work flexible hours, so whilst this is a convenient time for me to send this message I do not expect a response outside normal working hours.



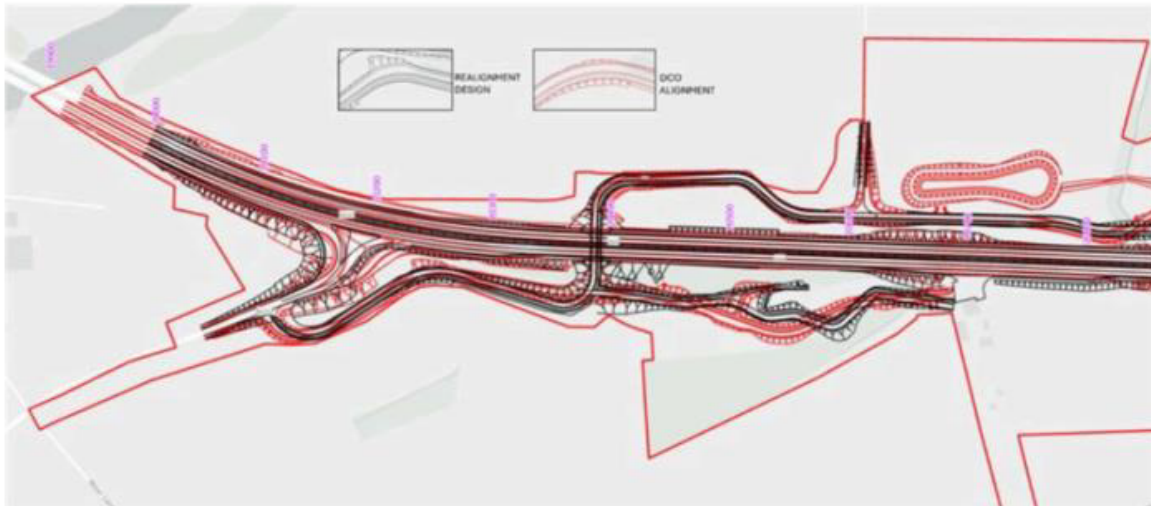
Subject: Re: A66 NTP Scheme 3a

INTERNAL

Hi [Redacted]

I understand your position on the status of the PMA and access rights for the public to the proposed car park at the Llama Karma Cafe site. We're committed to finding a solution and have been discussing ways to resolve with NH. At this stage we are just asking for the council to review the proposed changes to the DCO which are very minor and relate to the lateral position of PMAs outside of the Article 7 limits of deviation. [REDACTED] as detailed the reasons for the change below, which are all positive. The change doesn't affect the issue you're raising, which exists in the original DCO. We'd really appreciate it if you could review as soon as possible so we can progress with the change application. I've copied a screenshot showing the changes below.

Many thanks,

**AUREOS**aureos.com

Design Manager

[REDACTED]
[REDACTED]@aureos.com



REGISTERED OFFICE: 2nd Floor, One Lindsey Street, London, EC1A 9HP

CONFIDENTIALITY NOTICE: This email message and accompanying data are for the sole use of the intended recipient(s) and may contain information that is confidential. If you are not the intended recipient, you are notified that any use, disclosure, dissemination, storage, distribution or copying of this message or data is prohibited. If you received this email message in error, please notify us immediately and promptly erase all copies of this message and attachments.

Please note that Aureos Infrastructure Services Limited monitors incoming and outgoing mail for compliance with its Email policy. This includes scanning emails for computer viruses.

From: A66 NTP <A66NTP@westmorlandandfurness.gov.uk>

Sent: 08 August 2025 17:27

[REDACTED]@aureos.com>

[REDACTED]

Subject: RE: A66 NTP Scheme 3a

Hi [REDACTED]

Thank you for your email.

My initial view is that it does not make sense for the Council to agree a position of non-objection to a design which appears flawed in respect of the proposed PMA status and the need for public vehicular access to the Countess Pillar car park. However, we will discuss this at our project team meeting next week and come back to you.

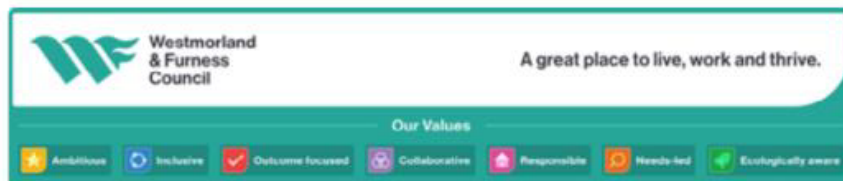
Kind regards

[REDACTED]

Manager (Major and Strategic Developments)
National Highways | Thriving Places
Westmorland and Furness Council | South Lakeland House | Lowther Street | Kendal | LA9 4DQ

[westmorlandandfurness.gov.uk](https://www.westmorlandandfurness.gov.uk)

Upcoming leave: 22nd August and 8th to 12th September 2025



Please be aware that I work flexible hours, so whilst this is a convenient time for me to send this message I do not expect a response outside normal working hours.



[REDACTED]

Subject: A66 NTP Scheme 3a

INTERNAL

Hi [REDACTED]

We are still requiring a letter of non-objection for the DCO NMA changes for Scheme 3a.

These changes refer to the B6262 Combined Public Rights of Way and Private Means of Access.

These drawings have been previously shared with [REDACTED] in January 2025 but have subsequently been updated (see attached).

I am aware there are ongoing queries from the council surrounding the PMA and access rights/parking at Llama Karma; however these queries exist on the DCO design and are therefore unrelated to the actual

DCO changes proposed.

These changes are minor and are proposed for the following reasons:

- Provide a safer arrangement by moving the PMA/B6262 junction further away from the B6262/A66 junction.
- Reduce work in the vicinity of scheduled monuments
- Reduce land take

We are happy for the non-objection to be caveated by yourselves in the letter.

We require this by next week, if you can please respond accordingly.

Kind regards,



AUREOS

aureos.com




Assistant Design Manager

@aureos.com



REGISTERED OFFICE: 2nd Floor, One Lindsey Street, London, EC1A 9HP

CONFIDENTIALITY NOTICE: This email message and accompanying data are for the sole use of the intended recipient(s) and may contain information that is confidential. If you are not the intended recipient, you are notified that any use, disclosure, dissemination, storage, distribution or copying of this message or data is prohibited. If you received this email message in error, please notify us immediately and promptly erase all copies of this message and attachments.

Please note that Aureos Infrastructure Services Limited monitors incoming and outgoing mail for compliance with its Email policy. This includes scanning emails for computer viruses.

Correspondence with landowners in relation to Proposed Change 1 Cross Street, Proposed Change 2 Green Lane, Proposed Change 3 Long Marton, Proposed Change 4 Access Track at Kirkby Thore, Proposed Change 5 Spitals Farm Access and Proposed Change 8 Powis House Private Means of Access

The following table lists all the individual landowners who were consulted for each of the Proposed Changes.

Annex C Reference	Landowner	Proposed Change(s)
C1.		1, 2 and 4
C2.		1
C3.		3
C4.		5
C5.		8
C6.		8
C7.		7
C8.		7

C1. John and Michael Metcalf Consultation Evidence – Proposed Changes 1, 2 and 4



Update meeting minutes			
Title:	Update meeting with [REDACTED] and [REDACTED]	Revision / Version:	Draft
Status:			
Date:	02/10/2025	Reason for Issue:	Information
Location:	Crossfell House Farm, Kirkby Thore	Time of Meeting:	2.30-3.30pm
Drafted By:	[REDACTED]		
Attendees:	[REDACTED]		
Apologies/ Unknown	[REDACTED]		
Resources			

Item	Description	Action	Responsible Person(s)
1	Going to be continuing with archaeology work and then doing some directional drilling for utility work. That should be spring next year in Cross Street. There is some archaeology work to do around Sanderson's Croft.		
2	Share pre-condition survey with MB	LL to send over	
3	Collapsed drainage outfall at Cross Street which needs to be repaired by Westmorland and Furness. Find out the latest	MM to pass to W&F	
4	MB and [REDACTED] confirmed happy with the amends to the PMAs.	MB will put this in writing too	
5	Concerns raised about the distance from the new wells by British Gypsum and the closeness to the slurry pit. It needs to be 50metres away from it.	LL and MM to look at the designs	
6	NoIE to be issued week commencing 8 October	MM to confirm with MB	
7	Pick up with NH about land	MM	

C2. British Gypsum Consultation Evidence – Proposed Change 1



RE: National Highways: A66 Northern Trans-Pennine Scheme - public means of access
CRM:0762257

From: [REDACTED] <[REDACTED]@pfk.co.uk>
Date: Thu 18/09/2025 4:33 PM
To: <A66 Northern Transpennine> <a66ntp@nationalhighways.co.uk>

 5 attachments (14 KB)

.....

Hi [REDACTED]

I have received instructions from British Gypsum regarding the amended overbridge location. In principle, they are happy with the proposed location; however, concerns have been raised regarding the type of surface proposed.

The rationale is that the track surface must be suitable for crane access to facilitate the removal of the boreholes in the future. The current plan shows the tracks as gravel, which may present access issues in the long term given the anticipated volume of farm traffic. A more durable surface, such as asphalt or concrete, would be preferable for long-term use.

Kind regards,

[REDACTED]

W: pfkrural.co.uk
10 The Courtyard, Edenhall, Penrith, Cumbria, CA11 8ST



To read PFKs legal notice, [click here](#)

From: A66 NTP <A66NTP@nationalhighways.co.uk>
Sent: 18 September 2025 08:11

Subject: National Highways: A66 Northern Trans-Pennine Scheme - public means of access CRM:0762257

Dear [REDACTED]

Please find attached two documents which we need you to review in relation to land owned by British Gypsum and Maurice Kennedy.

The image for British Gypsum shows us constructing the new Green Lane overbridge on the existing alignment.

The image for Maurice Kennedy shows the realignment of Long Marton to reduce the length of the bridge and the amount of land needed. This also allows us to reduce the speed limit to 50mph which will be done under a Traffic Regulation Order.

These are minor amends and although they are the plans we've been working from for some time we do need to share these with you. We also need to receive formal response from you to say you accept these changes by the end of next week if possible.

I've also attached the accommodation drawing which shows all these amends in action and how we've already been working through these with you. If you would like any further information or to discuss this over a Teams call then please do let us know.

Are you also able to send over the signed copies of the pre-condition surveys for [REDACTED] for our records.

Kind regards

[REDACTED]
Customer Lead – Balfour Beatty
A66 Northern Trans-Pennine project team
National Highways | Major Projects

Need to contact us?

Website: <https://nationalhighways.co.uk/our-work/a66-northern-trans-pennine/>

Email: A66NTP@nationalhighways.co.uk

Twitter – [www.twitter.com/A66NTP](https://twitter.com/A66NTP)

Facebook - <https://www.facebook.com/A66-Northern-Trans-Pennine-project-101479452278722/>

Post: A66 NTP, National Highways, Fifth Floor, 3 Piccadilly Place, Manchester, M1 3BN

Please let us know how we dealt with your query? [Link to Customer Survey](#)

This email may contain information which is confidential and is intended only for use of the recipient/s named above. If you are not an intended recipient, you are hereby notified that any copying, distribution, disclosure, reliance upon or other use of the contents of this email is strictly prohibited. If you have received this email in error, please notify the sender and destroy it.

National Highways Limited | General enquiries: 0300 123 5000 | National Traffic Operations Centre, 3 Ridgeway, Quinton Business Park, Birmingham B32 1AF |
<https://nationalhighways.co.uk> | info@nationalhighways.co.uk

Registered in England and Wales no 9346363 | Registered Office: Bridge House, 1 Walnut Tree Close, Guildford, Surrey GU1 4LZ

Consider the environment. Please don't print this e-mail unless you really need to.

C3. Maurice Kennedy Consultation Evidence – Proposed Change 3

----- Original Message -----

[REDACTED] pfk.co.uk>;

Received: Thu Sep 18 2025 16:07:06 GMT+0100 (British Summer Time)

To: <a66ntp@nationalhighways.co.uk>;

Subject: RE: National Highways: A66 Northern Trans-Pennine Scheme - public means of access CRM:0762257

H [REDACTED]

Thanks for the email.

[REDACTED] approval for the amendment has been granted, and they are happy with the proposed change.

British Gypsum: I have no objections, but I have forwarded this to my client for formal consent. I will let you know their response in due course.

AWJ – Pre-condition Surveys: I am satisfied with the surveys Lewis sent over for the Centre Pacts junction and believe I have already informed you of this. Please confirm whether I am required to print and sign on behalf of my client, or if written confirmation via email will suffice. If signatures are needed, I can arrange for them.

I also requested that [REDACTED] records of condition around Lightwater Cottages and the stone storage areas be re-done, as the quality of the original pre-condition survey was not acceptable. However, this may now be difficult given the drilling operations currently taking place.

Kind regards,

[REDACTED]
Rural Surveyor

PFK

[REDACTED]
W: pfkrural.co.uk

10 The Courtyard, Edenhall, Penrith, Cumbria, CA11 8ST



To read PFKs legal notice, [click here](#)

From: A66 NTP <A66NTP@nationalhighways.co.uk>

Sent: 18 September 2025 08:11

To: [REDACTED] <[\[REDACTED\]@pfk.co.uk](mailto:[REDACTED]@pfk.co.uk)>

Subject: National Highways: A66 Northern Trans-Pennine Scheme - public means of access CRM:0762257

Dear [REDACTED]

Please find attached two documents which we need you to review in relation to land owned by British Gypsum and [REDACTED]

The image for British Gypsum shows us constructing the new Green Lane overbridge on the existing alignment.

The image for [REDACTED] shows the realignment of Long Marton to reduce the length of the bridge and the amount of land needed. This also allows us to reduce the speed limit to 50mph which will be done under a Traffic Regulation Order.

These are minor amends and although they are the plans we've been working from for some time we do need to share these with you. We also need to receive formal response from you to say you accept these changes by the end of next week if possible.

I've also attached the accommodation drawing which shows all these amends in action and how we've already been working through these with you. If you would like any further information or to discuss this over a Teams call then please do let us know.

Are you also able to send over the signed copies of the pre-condition surveys for [REDACTED] for our records.

Kind regards

[REDACTED]
Customer Lead – Bairour Beatty
A66 Northern Trans-Pennine project team
National Highways | Major Projects

Need to contact us?

Website: <https://nationalhighways.co.uk/our-work/a66-northern-trans-pennine/>

Email: A66NTP@nationalhighways.co.uk

Twitter – [www.twitter.com/A66NTP](https://twitter.com/A66NTP)

Facebook - <https://www.facebook.com/A66-Northern-Trans-Pennine-project-101479452278722/>

Post: A66 NTP, National Highways, Fifth Floor, 3 Piccadilly Place, Manchester, M1 3BN


Please let us know how we dealt with your query? [Link to Customer Survey](#)

This email may contain information which is confidential and is intended only for use of the recipient/s named above. If you are not an intended recipient, you are hereby notified that any copying, distribution, disclosure, reliance upon or other use of the contents of this email is strictly prohibited. If you have received this email in error, please notify the sender and destroy it.

National Highways Limited | General enquiries: 0300 123 5000 | National Traffic Operations Centre, 3 Ridgeway, Quinton Business Park, Birmingham B32 1AF | <https://nationalhighways.co.uk> | info@nationalhighways.co.uk

Registered in England and Wales no 9346363 | Registered Office: Bridge House, 1 Walnut Tree Close, Guildford, Surrey GU1 4LZ

C4. Andrew Addison Consultation Evidence – Proposed Change 5

 Outlook

RE: National Highways: A66 Northern Trans-Pennine scheme - public means of access
CRM:0762255

 7 attachments (898 KB)

.....

I have spoken to [REDACTED] and he is pleased if you can move the side road closer to the main carriage way on the south side of the underpass as shown on the plans and confirms he agrees with this on the basis as always that his underpass access is maintained in terms of heights and width.

He has asked however if you can produce a large close in scale plan showing where your permanent boundary will be on the north side of the road around the farm steading, as he is thinking of expanding his buildings and could do with understanding where your final boundary will be to then decide what to do. Ideally if someone could go out and put some stakes in the ground on the north side to mark the boundary that would be even better. Could someone contact [REDACTED] and arrange this as well as sending a plan.

I look forward to hearing from you.

Regards

[REDACTED]
MRICS FAAV
Chartered Surveyor & Director of Professional Compliance

Please note my working days are Monday to Thursday



hhlandestates.co.uk

Borderway, Rosehill, Carlisle, CA1 2RS



[Click here](#) to see our disclaimer



RABI
Rural Access Initiative

RSABI
#KeepTalking

Proud to support our
chosen charities for 2025

From: A66 NTP <A66NTP@nationalhighways.co.uk>

Sent: 12 September 2025 13:14

To: [REDACTED]@hhlandstates.co.uk>

Subject: National Highways: A66 Northern Trans-Pennine scheme - public means of access CRM:0762255

Dear [REDACTED]

Please find attached two documents which we need you to review in relation to land owned by [REDACTED]

The image shows a change in the alignment of the new B6542 moving up to eight metres closer to the new A66.

This is a minor amend and although they are the plans we've been working from for some time we do need to share these with you. We also need to receive formal response from you to say you accept these changes in the next two weeks.

I've also attached the accommodation drawing which shows all these amends in action and how we've already been working through these with you. If you would like any further information or to discuss this over a Teams call then please do let us know.

I look forward to hearing from you.

Kind regards

[REDACTED]
Customer Lead – Bairour Beatty
A66 Northern Trans-Pennine project team
National Highways | Major Projects

Need to contact us?

Website: <https://nationalhighways.co.uk/our-work/a66-northern-trans-pennine/>

Email: A66NTP@nationalhighways.co.uk

Twitter – [www.twitter.com/A66NTP](https://twitter.com/A66NTP)

Facebook - <https://www.facebook.com/A66-Northern-Trans-Pennine-project-101479452278722/>

Post: A66 NTP, National Highways, Fifth Floor, 3 Piccadilly Place, Manchester, M1 3BN

Please let us know how we dealt with your query? [Link to Customer Survey](#)

This email may contain information which is confidential and is intended only for use of the recipient/s named above. If you are not an intended recipient, you are hereby notified that any copying, distribution, disclosure, reliance upon or other use of the contents of this email is strictly prohibited. If you have received this email in error, please notify the sender and destroy it.

National Highways Limited | General enquiries: 0300 123 5000 | National Traffic Operations Centre, 3 Ridgeway, Quinton Business Park, Birmingham B32 1AF |
<https://nationalhighways.co.uk> | info@nationalhighways.co.uk

Registered in England and Wales no 9346363 | Registered Office: Bridge House, 1 Walnut Tree Close, Guildford, Surrey GU1 4LZ

Consider the environment. Please don't print this e-mail unless you really need to.

C5. Colin Dent Consultation Evidence – Proposed Change 8



Title : Early work

Title:	Early work	Revision / Version:	Draft
Status:	Draft		
Date:	15/08/2025	Reason for Issue:	
Location:	Bridge End Farm	Time of Meeting:	1pm
Drafted By:			
Attendees:			
Apologies/ Unknown			
Resources			

Item	Description	Action	Responsible Person(s)
1	Milking of the cows takes place three times a day		
2	We'll be working in the field at the front of Powis House for archaeology, we'll be issuing a NoIE for that as its temporary land. Then we'll build the access track.	Send over temp fence plans to all land agents	LL
3	600 cattle in the fields and visit between 4-6 times a day then got crop.		
4	PMA happy with its depth even though it will flood as its easier to access the field. Aware that it will be slightly longer due to the viaduct being shortened. Happy with that.		
5	Site access will be from Long Marton road. Then move them on to the track once the viaduct is built but then access will be maintained where it is.		
6	Electricity pole will need diverting and water and BT got to move them into the track. Get specs for the tracks		
7	When they have taken off the last of the crop let us know	CD	
8	Utility work early next year then new track will be built and demolish the bungalow		
9	Need to move water troughs on the other side to the fields that are being returned.		
10	Borehole not being used at all but believe it to be near the bungalow.		

C6. Tim Nicholson Consultation Evidence – Proposed Change 8



Design updates and boundaries			
Title:	Design updates	Revision / Version:	Draft
Status:			
Date:	08/08/2025	Reason for Issue:	Information
Location:		Time of Meeting:	10am-1pm
Drafted By:			
Attendees:			
Apologies/ Unknown			
Resources			

Item	Description	Action	Responsible Person(s)
1	<p>Sleastonhow Lane</p> <p>Leave the tarmac area but the hedge will need to be removed as part of the temporary landtake. There is a small parcel of land which will be cut off which currently belongs to Britisg Gypsum could be worth a discussion about transferring ownership of that.</p> <p>The second gate looks be under the road – look at the design in more detail to understand that location.</p> <p>TN suggested cattle gates to prevent the cattle from getting along the track and out to the road. Will need to pick this up with Westmorland and Furness.</p>	Need to understand who will maintain the hedge along the lane	

Item	Description	Action	Responsible Person(s)
2	<p>Fields going towards the house Need to move water troughs when we take it as they feed both fields. Also need an additional gate adding – HC will send across marked up plan.</p> <p>Gates need to be towards the bottom corners of the fields as cattle will move downwards. This gates need to be moved to aid that process.</p> <p>The outfall will be fenced to prevent anything from getting stuck in it.</p>	<p>Will the hedge be returned?</p> <p>HC to send over marked up plans</p>	
3	<p>Track around the viaduct TN concerned about flooding as its close to the flood plain. Design details shows the track is just outside of this area and no issues have been identified. JB shared the design output.</p> <p>In the final design the path will be concrete on the section only used by TN. This will be unbound – JB to confirm this.</p> <p>It will be post and wire along the edge and owned by TN. There is a hedge running along the edge which will need to be fenced either side. Clippex fencing will be on the field side of the track.</p> <p>Track will be 4.2m but then has embankment either side of it as well.</p>	<p>Create cross section of the path showing the fencing etc either side</p>	
4	<p>Open ditch TN doesn't want to maintain the ditch. NH own the culverts and will need to access to maintain them.</p> <p>NH will own the track down to the viaduct and the pond and this track will be Tarmac. Discussion about who owns the track and who grants rights to be had.</p> <p>JB said discussion about whether this should be a ditch or a pipe but would prefer a ditch.</p>		

Item	Description	Action	Responsible Person(s)
5	<p>Construction and fence lines Will build the new lane and bridge first then move to the viaduct. Most of the vehicles and materials for the viaduct will come from Long Marton and access over a river bridge.</p> <p>Need to understand how the fields will be run as we'll need to put in a diversion while we create the new lane and bridge.</p> <p>The third piece of land – continue to operate until we're ready to take it as this will be the last activity.</p> <p>Temp fencing needs to have sheep netting and electric top wire. If it is Clippex fencing then happy for this to be installed as the beginning, if its post and wire then this needs to be temp and replaced at the end.</p> <p>Need to be aware of troughs, two man hole covers and borehole – a must to keep this operating.</p> <p>New electricity – marked up on the ground for TN and send spec to HC. Will need able to keep the farm operating as its used every day.</p> <p>Some archaeology mitigation to do in the top corner of the field where the viaduct is. Oxford Archaeology looking to do that early spring or summer.</p> <p>TN confirmed he could be disrupted for a couple of hours or even a day if he knows about it in advance but would need compensating if any longer.</p>	LL to provide more details about the diversion around the lane and discuss further	
6	<p>No through route There is a sign to prevent traffic from going up through Kirkby Thore. TN is concerned this may deter vehicles from coming into the farm at all with deliveries as well.</p>	Look at this design	
7	<p>Public rights of way Around 14 will be rediverted but any that are impacted short term will need to have diversions</p>		
8	<p>Bund Agree with 2 metres higher then lose 14 metres of the land.</p> <p>IC and JB explained that by moving it closer to the property they will hide the viaduct but TN also concerned it also hides all the view.</p>	TN to discuss with Emma and Felicity	

Item	Description	Action	Responsible Person(s)
5	<p>Commitments to date</p> <p>Fencing will be Clippex with metal posts on land boundary, second fence will be post and wire</p> <p>All specialised fences will be maintained by NH</p> <p>Laybys have been moved away from the property</p> <p>Bund will have hedge and grassland on the top</p> <p>Passing places provided</p> <p>Track will be more than 4 metres wide and unbound on the sections maintained by TN</p> <p>Rabbit fencing is needed for where the cycle track used to be</p>		

Correspondence with landowners in relation to Proposed Change 7 B6262 Combined Public Rights of Way and Private Means of Access

C7. [REDACTED] Consultation Evidence – Proposed Change 7

From:
Sent:
To:
Subject:

Attachments:

[WARNING] This email is from outside of the organisation. Do not open attachments or links if you are unsure of the source.

[Block sender](#) | [Report](#)

[REDACTED]

We met with [REDACTED] on Friday evening and we can confirm that our client is content with the amendments which are proposed by the plan attached.

I trust that this is satisfactory for your purpose.

Regards

[REDACTED]

[REDACTED]

INTERNAL

H [REDACTED]

Thank you for taking the time to talk to me on Teams this afternoon.

We spoke about the non-material change to the DCO, and particularly the changes to the private means of access east of the B6262. We outlined the changes that are formally laid out below in the email from our assistant design manager, [REDACTED]

We would be grateful for confirmation that these amendments to the DCO will be acceptable to [REDACTED]

Kind Regards

[REDACTED]

AUREOS

aureos.com



Lead

com



REGISTERED OFFICE: 2nd Floor, One Lindsey Street, London, EC1A 9HP

CONFIDENTIALITY NOTICE: This email, message and accompanying data are for the sole use of the intended recipient(s) and may contain information that is confidential. If you are not the intended recipient, you are notified that any use, disclosure, dissemination, storage, distribution or copying of this message or data is prohibited. If you received this email message in error, please notify us immediately and promptly erase all copies of this message and attachments.

Please note that Aureos Infrastructure Services Limited monitors incoming and outgoing mail for compliance with its Email policy. This includes scanning emails for computer viruses.

Subject: RE: Action needed: A66 Northern Trans-Pennine Project, Non-Material Amendment Application, Change 7 - B6262 Combined Public Rights of Way and Private Means of Access, PMA References 8, 10, 11 CRM:0157022

INTERNAL

Please see below sketch showing the proposed alignment differences for Change 7, and a description of the change.

Happy to have a call to discuss if needed.

Description of the proposed change

- Realign the PMA junction with the B6262
- Realign the PMA South of the A66 between Ch 80-140 & Ch 220-300
- Realign the PMA North of the A66 between Ch 80-100 & Ch 475-500
- Realign the PMA to Llama Karma Kafe

The reason for the proposed change

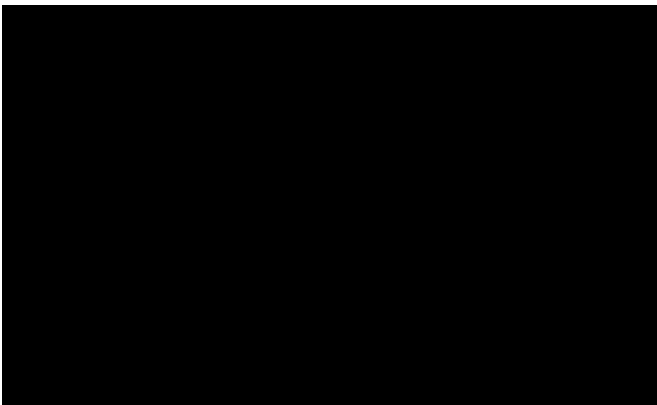
The design has been optimised between the B6262 side road and the Lightwater Watercourse to:

- Provide a safer arrangement by moving the PMA/B6262 junction further away from the B6262/A66 junction.
- Reduce work in the vicinity of scheduled monuments.
- Provide sufficient space for boundary treatments, drainage and Vehicle Restraint Systems whilst avoiding the need to construct a retaining wall.
- Increase the amount of land given back to the landowner.
- Provide a more direct route to the Llama Karma Kafe.

- Reduce vehicle speeds across the Brougham Overbridge.
- Optimise the crossing of the Lightwater watercourse to reduce construction costs.



Kind regards



C8. [REDACTED] Consultation Evidence – Proposed Change 7

[WARNING] This email is from outside of the organisation. Do not open attachments or links if you are unsure of the source.

[Block sender](#) | [Report](#)

[REDACTED]

I can confirm the amendments to the DCO are acceptable with my client.

Kind regards,

[REDACTED]

W: pfkrural.co.uk

10 The Courtyard, Edenhall, Penrith, Cumbria, CA11 8ST



To read PFKs legal notice, [click here](#)

[REDACTED]

Thank you for taking the time to talk to me on the phone this afternoon. We spoke about the non-material change to the DCO, and particularly the changes to the private means of access east of the B6262, crossing the overbridge, and the route along the north side). However, these changes are formally laid out in the email from our assistant design manager [REDACTED] below.

We would be grateful for confirmation that these amendments to the DCO will be acceptable to [REDACTED]

Kind Regards

[REDACTED]

AUREOS

aureos.com



[REDACTED]
Customer & Social Value Lead

[REDACTED] aureos.com



REGISTERED OFFICE: 2nd Floor, One Lindsey Street, London, EC1A 9HP

CONFIDENTIALITY NOTICE: This email message and accompanying data are for the sole use of the intended recipient(s) and may contain information that is confidential. If you are not the intended recipient, you are notified that any use, disclosure, dissemination, storage, distribution or copying of this message or data is prohibited. If you received this email message in error, please notify us immediately and promptly erase all copies of this message and attachments.

Please note that Aureos Infrastructure Services Limited monitors incoming and outgoing mail for compliance with its Email policy. This includes scanning emails for computer viruses.

[REDACTED]

Subject: RE: Action needed: A66 Northern Trans-Pennine Project, Non-Material Amendment Application, Change 7 - B6262 Combined Public Rights of Way and Private Means of Access, PMA References 8, 10, 11 CRM:0157022

INTERNAL

[REDACTED]

Please see below sketch showing the proposed alignment differences for Change 7, and a description of the change.

Happy to have a call to discuss if needed.

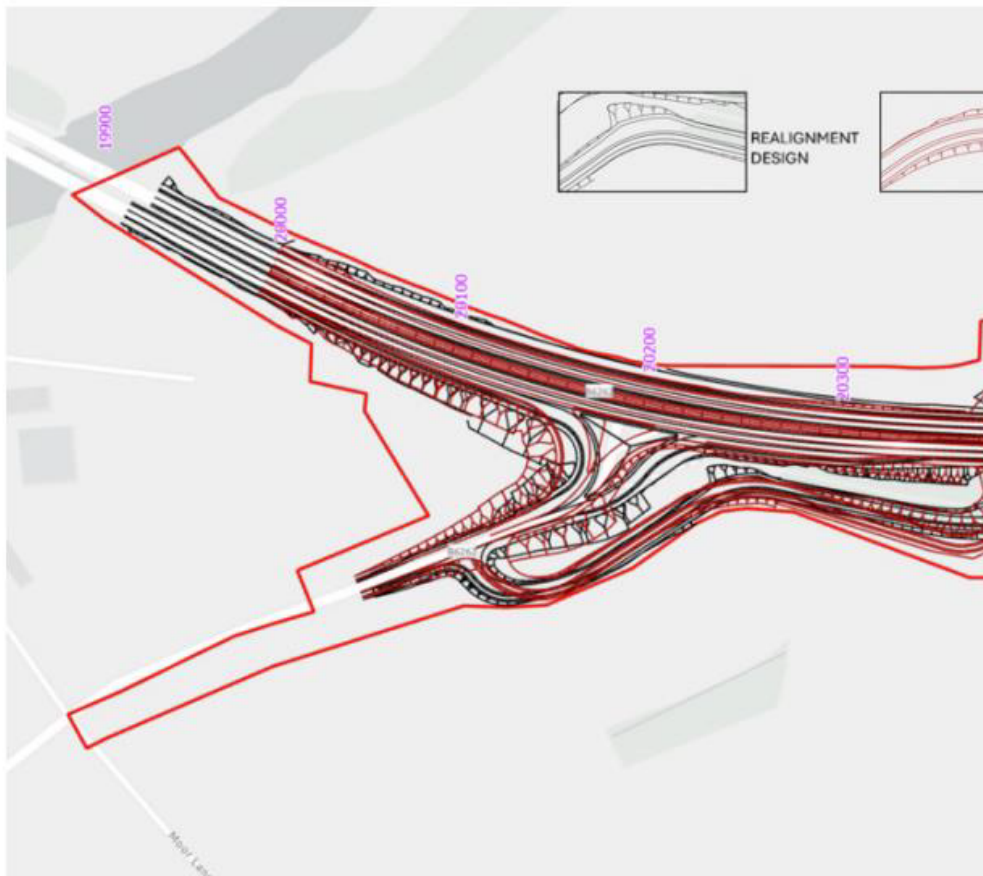
Description of the proposed change

- Realign the PMA junction with the B6262
- Realign the PMA South of the A66 between Ch 80-140 & Ch 220-300
- Realign the PMA North of the A66 between Ch 80-100 & Ch 475-500
- Realign the PMA to Llama Karma Kafe

The reason for the proposed change

The design has been optimised between the B6262 side road and the Lightwater Watercourse to:

- Provide a safer arrangement by moving the PMA/B6262 junction further away from the B6262/A66 junction.
- Reduce work in the vicinity of scheduled monuments.
- Provide sufficient space for boundary treatments, drainage and Vehicle Restraint Systems whilst avoiding the need to construct a retaining wall.
- Increase the amount of land given back to the landowner.
- Provide a more direct route to the Llama Karma Kafe.
- Reduce vehicle speeds across the Brougham Overbridge.
- Optimise the crossing of the Lightwater watercourse to reduce construction costs.



Kind regards

